



CITY WEST TEXT AMENDMENT

CITY WEST TEXT AMENDMENT 18.296

Public hearing of the staff-initiated request for Planning Commission's recommendation to City Council regarding Text Amendment 18.296 in order to create a new zoning district, CW City West, with associated changes to reference the new district as appropriate, and to amend text related to the EXO District, to reflect an underlying zoning of CW - City West.

REQUIRED ACTION

Recommendation to City Council regarding Text Amendment 18.296 to CW City West.

MOTION SHEET

Approval- Text Amendment

In the matter of Text Amendment 18.296, motion to **recommend approval** to City Council to adopt the City West District into the Zoning Ordinance as shown in the draft amendment *for the following reasons*:

1. *The 2016 Master Plan for Land Use recommended the creation and adoption of a new zoning district for this area of the City in order to foster redevelopment of underutilized parcels, and to create a vibrant, walkable, mixed-use district,*
2. *The Master Plan for Land Use objective to foster a favorable business climate is fulfilled by allowing more flexible development standards for a unique area of the City,*
3. *The Master Plan for Land Use objective to support and strengthen existing businesses and attract new businesses is fulfilled by allowing existing businesses to expand and creating new development opportunities in a mixed-use environment,*
4. *The Master Plan for Land Use objective to provide a wide range of housing options is supported as the new district allows residential use in a mixed-use setting,*
5. *The Master Plan for Land Use objective to develop the City West/Grand River and Beck area in a manner that supports and complements neighboring areas through the use of setback and height restrictions to provide buffers to single family districts, and*
6. *It provides an opportunity for long-standing businesses to remain at their current location.*

-OR-

Denial – Text Amendment

In the matter of Text Amendment 18.296, motion to **recommend denial** to City Council *for the following reasons ...*

MAPS

Location: 2020 Aerial

2005 Aerial

1990 Aerial

1974 Aerial

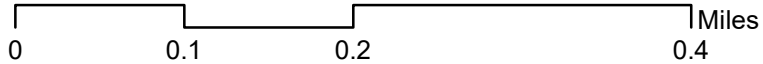
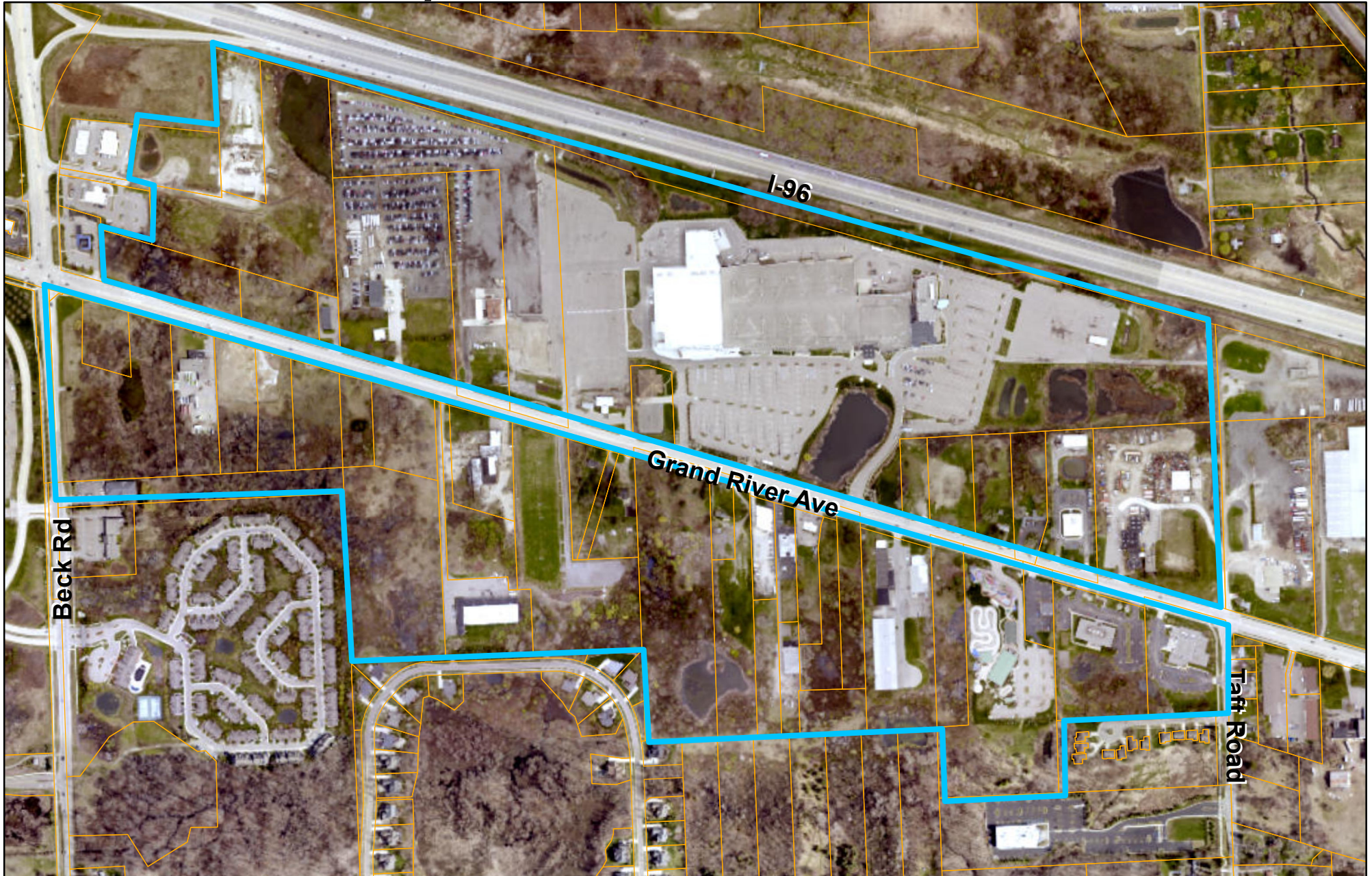
1949 Aerial

Current Zoning

Future Land Use

Natural Features

City West District: 2020 Aerial



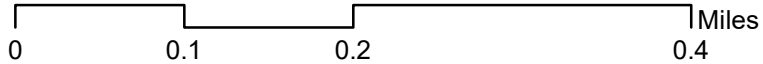
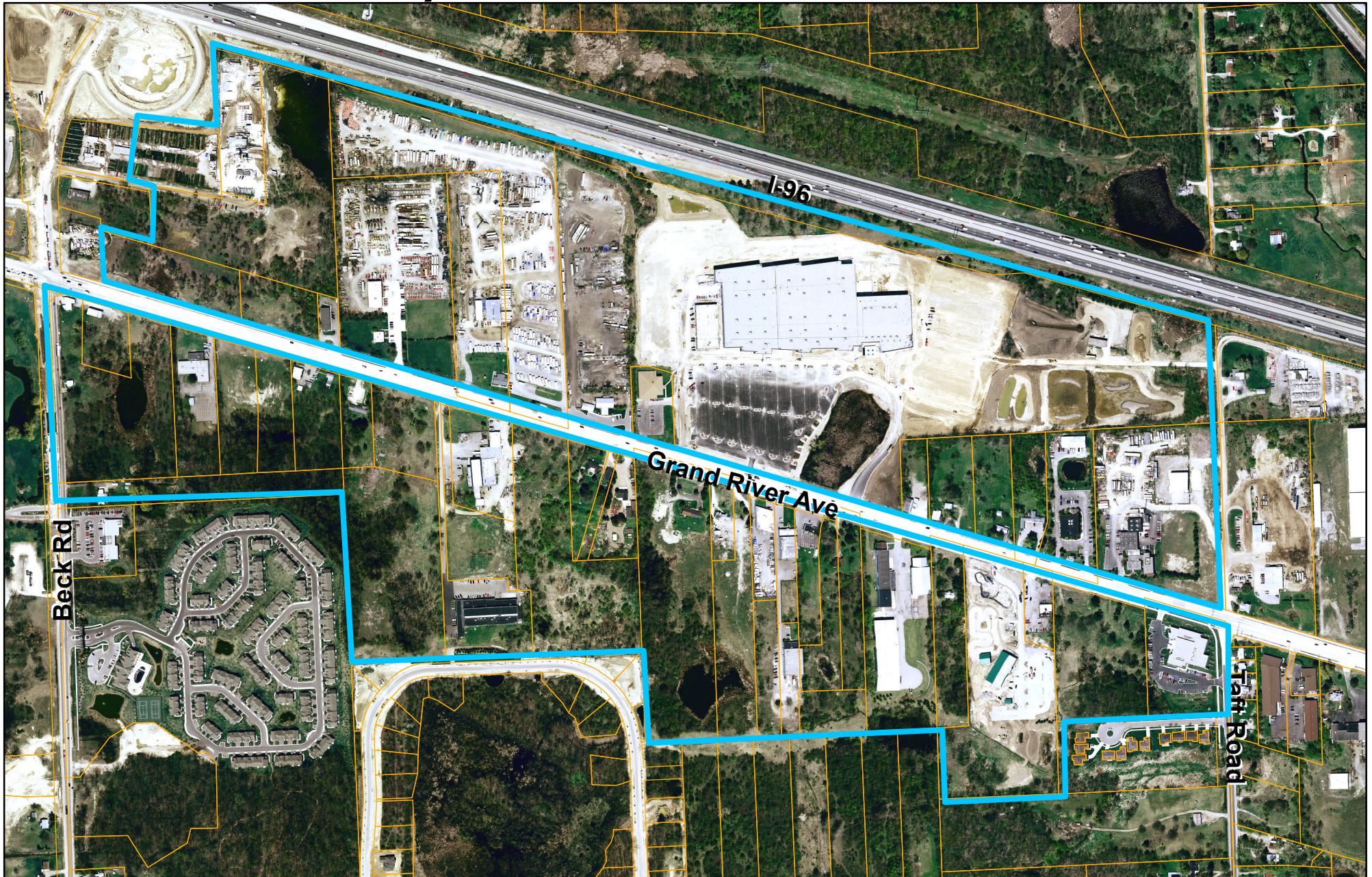
MAP INTERPRETATION NOTICE

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Map Author: Lindsay Bell
Date: 5/1/23
Project: City West

City West District: 2005 Aerial

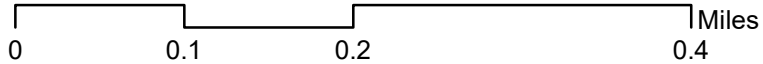


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City West District: 1990 Aerial



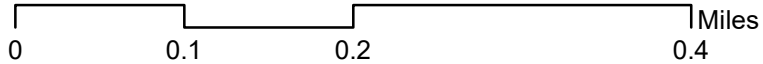
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Map Author: Lindsay Bell
Date: 5/1/23
Project: City West

City West District: 1974 Aerial

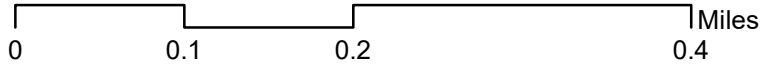


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City West District: 1949 Aerial



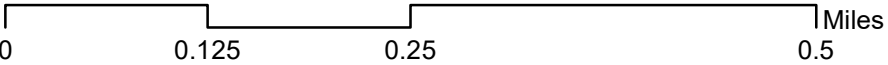
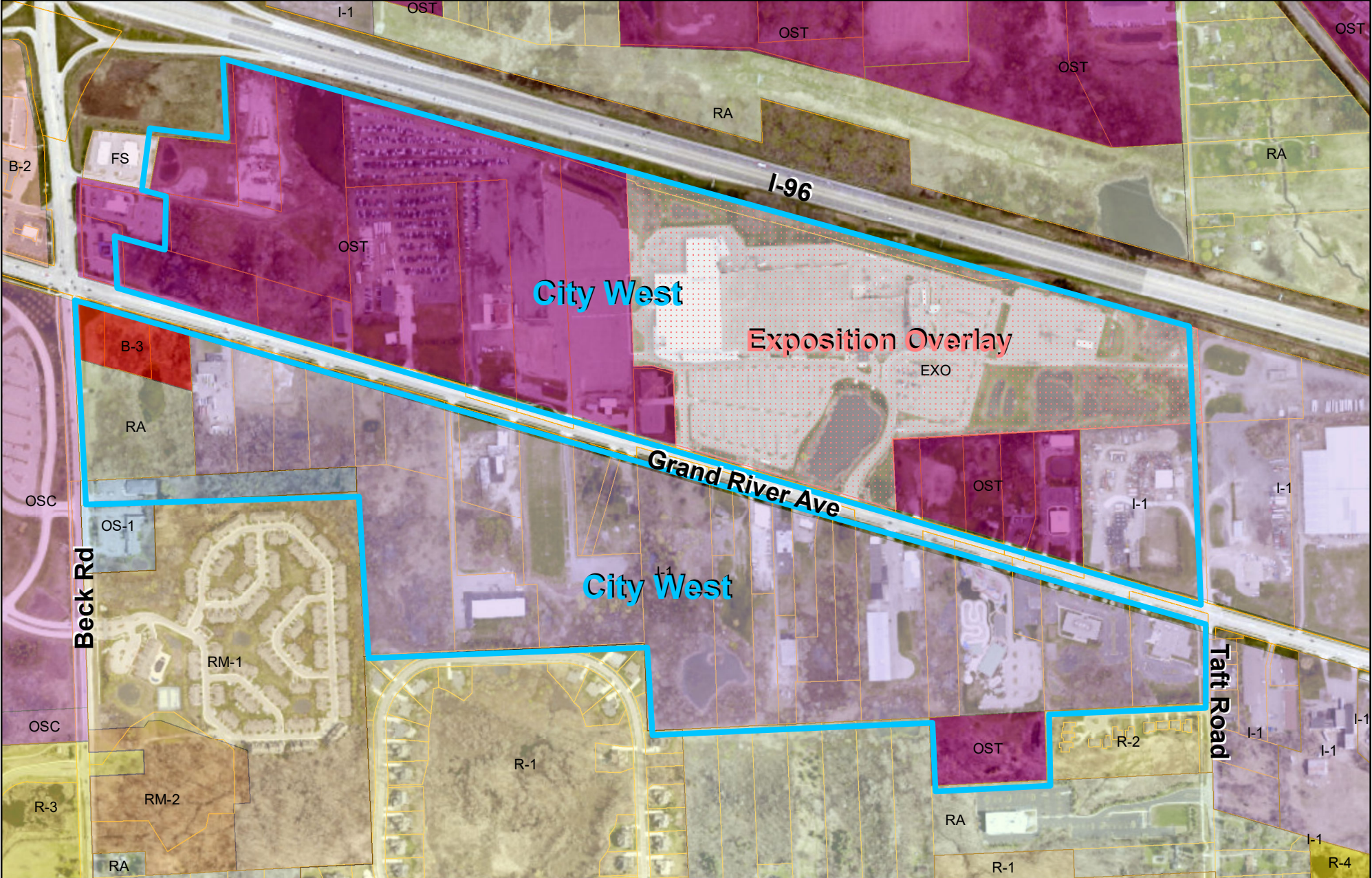
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Map Author: Lindsay Bell
Date: 5/1/23
Project: City West

City West District: Current Zoning



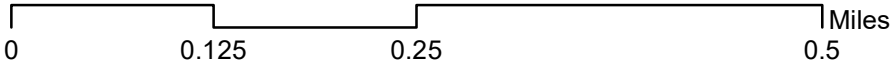
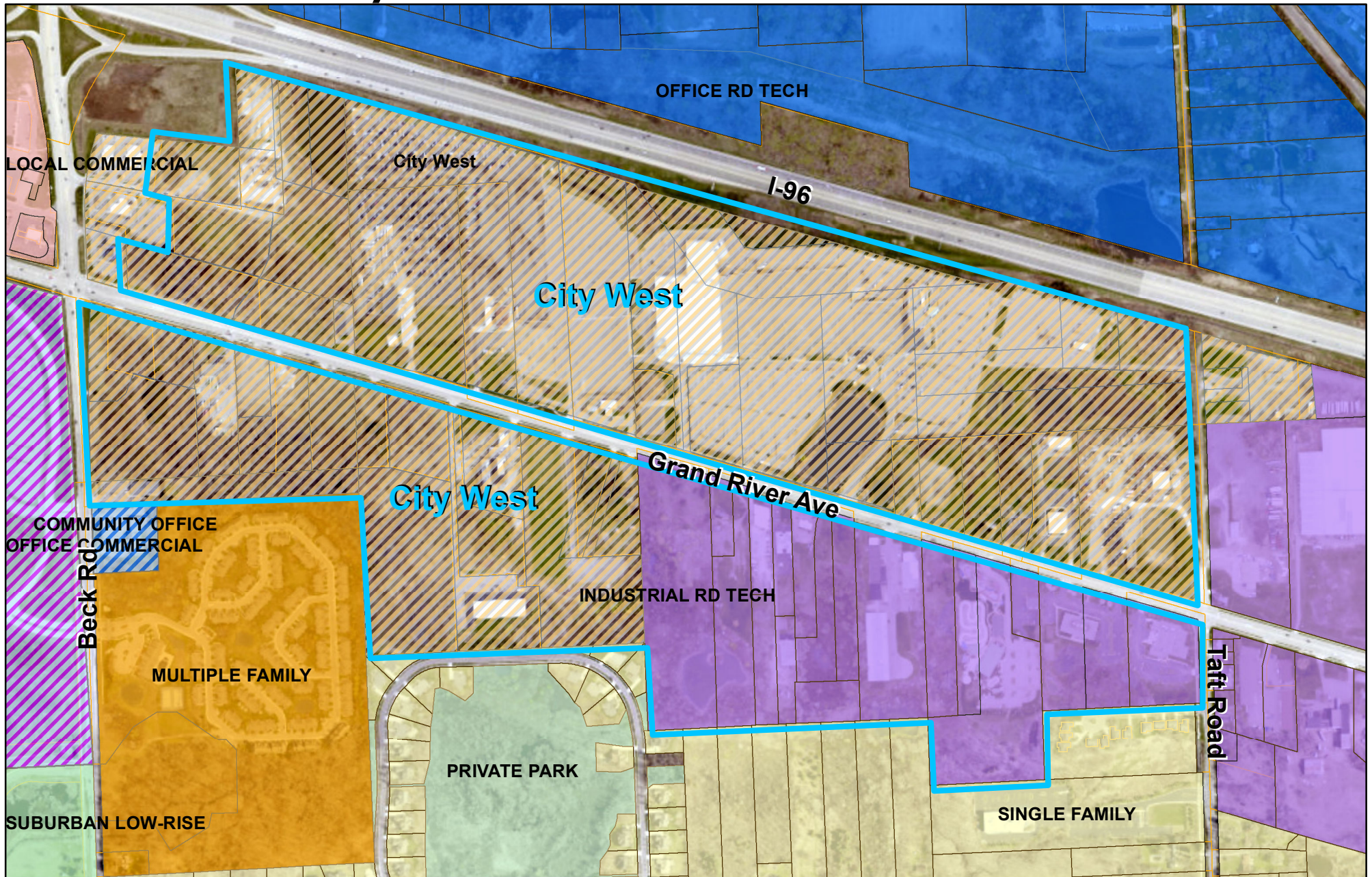
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Map Author: Lindsay Bell
Date: 3/38/23
Project: City West

City West District: Future Land Use



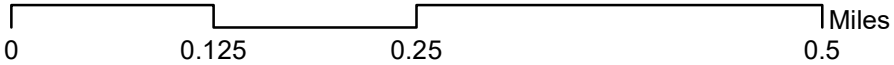
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Map Author: Lindsay Bell
Date: 3/38/23
Project: City West

City West District: Natural Features



Legend

-  Wetlands
-  Woodlands

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PLANNING MEMO

MEMORANDUM



TO: PLANNING COMMISSION
FROM: LINDSAY BELL, AICP, SENIOR PLANNER
THROUGH: BARBARA MCBETH, AICP, CITY PLANNER
SUBJECT: TEXT AMENDMENT 18.296, CITY WEST DISTRICT
DATE: MAY 5, 2023

BACKGROUND

In 2016 there was a push, from City Administration, to identify sites considered vacant or under-utilized, throughout the City that were ripe for redevelopment. Those sites would eventually be included in the City's 2016 Master Plan Update. Ultimately, three sites were selected based on their specific location and unique size and features. One of the sites identified was named City West as it on the west side of Novi along the north and south sides of Grand River Avenue between Beck Road and Taft Road. Over the last several years staff has been working to create a new zoning district that would help to transform this area from the sparsely developed industrial past to a vibrant, walkable, mixed-use district described in the Master Plan.

Working with the Planning Commission's Implementation Committee, planning staff and the City Attorney's office have developed new City West Zoning Ordinance text to provide the standards under which properties in the new district will be developed, as well as a Design Guide that offers visual examples that the Master Plan and ordinance language are working together to accomplish. The Design Guide complements the ordinance language and will be made part of the ordinance by reference. Properties to be included in the City West District would be considered as a part of a city-initiated rezoning that follows the public hearing and notice standards of a typical rezoning request.

The ordinance contains provisions for the existing uses that may choose to remain as is once the properties are rezoned, and not redevelop under the new standards. The ordinance will consider the uses existing at the effective date of the ordinance as legal non-conforming uses that may continue to be used in the same manner as allowed throughout the City.

This memo provides the following:

- An overview of the proposed district (see attached maps),
- A description of the existing character of the district,
- Historical context and Existing structures,
- The stated intent of the proposed district,
- An overview of the ordinance standards, including the proposed Mixed-Use Development Option (MDO),
- Buffering standards for properties adjacent to existing residential areas,
- Feedback from property owners in the district and adjacent, and

- Next steps for the draft ordinance and design guide.

EXISTING CONDITIONS

North Side of Grand River Avenue

The area identified as City West in the Master Plan is approximately 200 acres. Of that area, approximately 144 acres are north of Grand River Avenue with 21 parcels included. The majority of the area, 128.13 acres, is currently zoned OST Office Service Technology. There is an EXO Exposition Overlay District over a 55-acre portion of the Suburban Collection Showplace property. Approximately 14.06 acres are zoned I-1 Light Industrial. A mix of offices, industrial and manufacturing businesses, and vacant properties are present on either side. It is recommended that the three parcels fronting on Beck Road are excluded from the City West rezoning (i.e., Chase Bank, Starbucks, and USA 2 Go), as they are more recent developments containing drive-through uses that are generally not consistent with the intent of the district, but serve a convenience purpose for customers utilizing the highway.

South Side of Grand River Avenue

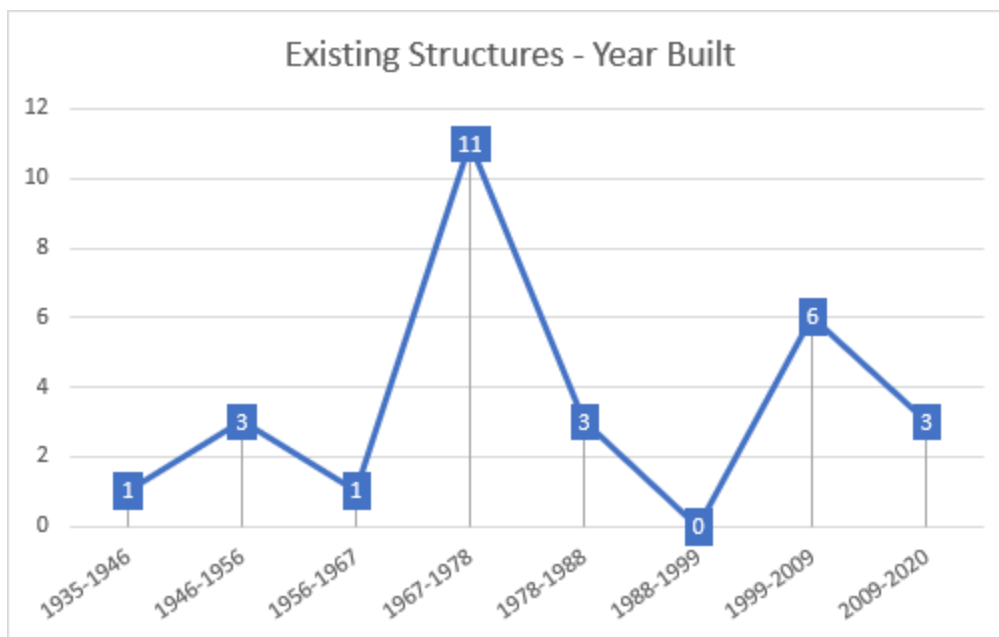
To the south of Grand River Avenue, the City West area is recommended to include all of the parcels between Beck Road and Taft Road. The vision of the Master Plan had included only the half-mile east of Beck Road to be included in the new district, but in working with the Implementation Committee, it was suggested that the district extend the entire mile along the south side of Grand River from Beck to Taft Roads. The district on the south side of Grand River includes 33 parcels totaling approximately 113 acres. The current zoning of the area is mostly I-1 Light Industrial. One area located at the southeast corner of the intersection of Beck Road and Grand River Avenue is zoned B-3. Fronting on Beck Road, one 6.4 acre area is zoned RA Residential Acreage, and a 3.4 acre parcel is zoned OS-1 Office Service. There are some significant areas of wetland in the southern portion of the district. Preserving those wetlands would help to provide natural buffers between the City West developments and the residential districts to the south.

HISTORICAL CONTEXT

Historical photos of the area show the land was largely agricultural fields and orchards in the 1940s. When I-96 was constructed in 1957 there was a highway rest stop until the Suburban Collection Showplace was constructed in 2005 in its location. The Fairlane Motel appears to have been developed on the north side of Grand River in the 1950s, and is still open today. By the 1970s there was greater industrial activity in the area, including the Wix-Mix concrete plant, trucking operations, outdoor storage yards, various industrial buildings, and a gas station near the intersection of Beck and Grand River. Over the last 20 years many of the remaining homes and vacant structures have been torn down after falling into disrepair, and the properties have not yet redeveloped.

EXISTING DEVELOPMENT

Of the existing structures in the area, the average age is 43 years, with many built between 1967 – 1978. Excluding building additions, only two new projects have been developed over the last 10 years: PetSuites (2020) and Beck Plaza, which contains the Starbucks (2016). During this same period development has not lagged in the rest of the City.



The number of permitted uses under I-1 zoning is limited when adjacent to single-family districts, which may have contributed to the lack of investment in this area. Restaurant uses in the I-1 district are only permitted in limited circumstances, so the existing restaurants south of Grand River have been non-conforming uses and have not been permitted to expand or make desired improvements such as adding covered outdoor patio areas. The expense of extending public utilities may have also discouraged new development.

Approximately 110 acres in the district are vacant or have a non-conforming residential use. Parcels developed with industrial uses that are not consistent with the desired future use for City West make up a total of about 32 acres. Development nearby in the corridor includes destinations such as Ascension Providence Hospital System and Suburban Collection Showplace, which are significant economic drivers in Novi, providing jobs and bringing in visitors. Commercial uses have developed by the I-96 interchange at Beck Road north of Grand River.

BASIS FOR THE NEW ORDINANCE LANGUAGE

The 2016 Master Plan Update recommended the creation of a cohesive district that supports long-term vitality and projects a sense of place. “A City West district should use

form-based code elements to guide the development of a dense, walkable, unified district featuring a mix of arts, entertainment, retail, restaurant, hotel, convention/exposition, office and residential uses. Standards for streetscape design, sidewalks, public amenities, and structured parking should all be considered. A sub-district permitting different maximum building heights should be established, permitting lower heights south of Grand River, and higher maximums near I-96."

The vision described in the 2016 Master Plan recommended that the City West district north of Grand River would be the highest intensity district in the City of Novi, with buildings up to ten stories and residential density of 30 units per acre. Previous Master Plan open house events garnered comments from both retirees and millennials wanting housing opportunities that would allow them to walk or bike to activity and shopping destinations and within developments that increase the vitality of Novi.

As the current 2023 Master Plan update effort continues, Novi continues to grow, as evident from the 20% population growth from 2010-2020. At the recent community open houses, 74% of participants indicated the City West area should be "transformed" when given the definition "long-term and large-scale change in the appearance and function of an area with the intent for dramatic shift in use, design, and accessibility." Another 21% indicated the area should "evolve," while only 5% thought the area should be maintained in its current state. The City West area provides an opportunity to expand the housing choices available for young professionals, empty nesters, and other residents who prioritize a more urban, walkable community, easy access to the highway and destinations within Novi, and entertainment opportunities. Grand River Avenue is a major corridor with greater road capacity than many areas of the city.

Based on the guidance in the 2016 Master Plan, and using the text of the Town Center districts as a model, staff developed the first draft Ordinance to achieve the type of development envisioned for this area. Over time, the City West ordinance has evolved to incorporate elements of the GE Gateway East and the Planned Development options as well. The City West District includes a baseline level of development for principal permitted uses, as well as an optional Mixed-Use Development Option (MDO) which permits a wider range of uses and higher intensity development in order to encourage the creation of a dynamic mix of compatible uses.

While the MDO provides greater flexibility in parking and landscaping, as well as setbacks and building height to allow a more urban form of development, projects will still be subject to wetland and woodland protection ordinances. Many of the existing wetland and woodland areas are adjacent to the southern portion of the district, where preservation would create a natural buffer area to the adjacent single-family districts. One of the project criteria to be evaluated is whether a proposed site design and layout minimizes negative impacts on existing natural features. Other criteria provide stipulations that surrounding

landowners shall not be unreasonably burdened, and the proposed development is deemed to be compatible/harmonious with surrounding area.

At the baseline level of development, building height is limited to three (3) stories. While the proposed ordinance standards allow for greater height along the north side of Grand River for MDO projects, staff believes that most projects will have 3-5 story buildings, with additional height potentially requested for mixed-use buildings, or for a signature office building or hotel. The maximum height is limited to eight (8) stories in the proposed ordinance for developments on the north side of Grand River Avenue, and five (5) stories on the south side of Grand River Avenue.

The proposed provisions permitting higher buildings involves meeting specific criteria:

- Preservation of open space (25% of site or more)
- Using sustainable building elements and design strategies (ex: LEED Best Practices)
- Allocation of units for Workforce Housing (at least 15%)
- Lower-level parking or parking garage to limit impervious surfaces

INTENT AND GENERAL DESCRIPTION

The following is the description and stated intent of the district from the draft ordinance:

1. *Description of the District. The City West district permits a mix of uses, including amusement and entertainment uses, office, commercial, and multiple-family housing, that are appropriate within the context of the City West area. Key design concepts include multiple-story buildings that frame the internal street network, a corridor of buildings massed along Grand River Avenue, shared access and parking, a mix of uses including commercial and multi-family residential, and public gathering spaces. Buildings should have high-quality materials and allow for changing uses over time. Development will be pedestrian-oriented, with a high degree of transparency on the ground level and functional, attractive outdoor amenities such as plazas and pocket parks with seating and lighting. Parking is placed to the side or rear of main buildings, preferably in parking structures, driveways are consolidated from the major thoroughfare, and on-street parking is permitted on secondary streets.*
2. *Intent. The intent of the City West district is to encourage high-quality and distinctive development that will complement and support nearby areas. The district will reflect an inviting and vibrant identity for this gateway to the City's Grand River Corridor area while implementing land use planning and development techniques that result in a pleasing, compatible, visual presence. Assembly of small lots and long, narrow lots into larger development parcels is anticipated and encouraged to provide coordinated development with consolidated access points. Minimum lot and frontage thresholds are established for the mixed-use development option to further this objective. Shared parking is strongly encouraged, and vehicular and pedestrian connections between sites is expected.*

The district allows a wide range of principal uses. In conjunction with different height, area, and bulk standards, this should increase development and re-development opportunities that will look demonstratively different than existing uses.

Additionally, some of the objectives of the district are intended to be accomplished or facilitated by the establishment of an optional development approval process, the Mixed-Use Development Option (MDO). Under this optional form of development:

- *Mixing of uses is permitted, either vertically or horizontally.*
- *Building heights are greater than most traditional commercial districts in the city.*
- *Floor area ratios and densities are increased to allow economies of scale.*
- *Required setbacks are reduced to encourage a development pattern that frames the primary street frontage.*

Under the MDO concept, a wider variety of retail, commercial, office, and residential uses and open spaces are permitted, to further the creation of a dynamic mix of compatible uses. As a trade-off for this greater flexibility, regulations regarding streetscape and landscape design, provisions for parking facilities, architectural and façade design, multi-family residential dwelling units, and setback standards are incorporated into the district, resulting in a mix of some form-based standards and more conventional guidance. See the City West Design Guide for examples of the development vision and guidelines for projects in this district. The MDO, if successfully implemented, should:

- *Encourage the use of land in accordance with its character and surroundings;*
- *Conserve natural resources and natural features;*
- *Encourage innovation and creativity in land use planning;*
- *Provide enhanced housing, cultural, and recreational opportunities for the people of the City; and*
- *Bring about a greater compatibility of design and use between and among neighboring properties.*

RESIDENTIAL AND MIXED-USE DEVELOPMENT OPTIONS

Residential uses can be developed under the MDO, either as a stand-alone use or as part of a mixed-use project. The granting of a Mixed-Use Development Option application requires review and recommendation by the Planning Commission and approval of the City Council. As part of the review and approval process, and in light of the potential additional uses and flexible building standards, the development may be subject to certain conditions, and will have to demonstrate conformance with the Design Guide adopted by the City as part of this ordinance. The MDO process is similar to the approval process for the PD-1 and PD-2 Options, with City Council authorized to grant deviations to area, bulk, yard and dimensional requirements, as well as density, if found to be an enhancement in the public interest and to not be detrimental to the natural features and resources of the affected property and surrounding area, or would enhance or preserve such natural features and resources.

The City West area south of Grand River Avenue is directly adjacent to areas designated for multi-family and single-family residential. This portion of the district is intended to provide a moderate-density transition to the surrounding uses, with buildings between two (2) to five (5) stories tall. Within 100 feet of existing single family residential areas to the south, non-residential uses are not permitted, and residential buildings are limited to 35 feet in height to ensure a proper transition and prevent undue impact on the existing neighbors. The 100-foot setback for non-residential uses is consistent with current I-1 District requirements, to insure that no new hardship is created on landowners, and residents are afforded the same protections that exist elsewhere. Residential uses are required to be set back a minimum 2 feet for each foot of building height. If the maximum building height of 35 feet is proposed, that structure would require a minimum 70-foot setback from the single-family district. By way of comparison with existing multiple family zoning districts in Novi, in the RM-1 and RM-2 Districts, the minimum side and rear yard setbacks are 75 feet. The City West district also requires a 50-foot setback for any parking areas adjacent to single family zoning districts, as well as the installation of a wall or landscaped berm to provide buffers. The landscape ordinance requirement for a 6 to 8 foot screening wall or berm would also apply where adjacent to single family districts.

Development in this district is required to meet specific design standards to ensure future development is cohesive and walkable. Buildings are meant to front on internal street networks rather than Grand River, which would be designed to create a more pedestrian-friendly development than the thoroughfare can offer. Shared off-street parking facilities are encouraged. Development projects are required to provide public plazas and open spaces for gathering.

The creation of the new district also requires some amendments to other parts of the Zoning Ordinance, especially for various Use Standards in Chapter 4, and for the sections related to the EXO Exposition Overlay District. As we are proposing to change the underlying zoning to City West, that requires changes also be made to the description and permitted uses of the EXO, and the required conditions, while still allowing the exposition facility to remain and expand, as needed.

PUBLIC FACILITIES

The historic Grand River Avenue corridor, running from downtown Detroit all the way to Lake Michigan on the west side of the state, bisects the City West area. In this area of the city, Grand River is an auto-centric five-lane major arterial with a speed limit of 50 miles per hour. The road characteristics make the streetscape inhospitable for pedestrians and cyclists. In order to create a more walkable environment, staff would recommend the City appeal to the Road Commission for Oakland County (RCOC) to reduce the speed limit in this area,

which is not uncommon in other parts of the corridor with greater development intensity, including the Town Center area where the speed limit is 40 miles per hour.

A mid-block crossing would also be advisable to provide another option for pedestrians and bicyclists to get between the north and south sides of the district as marked crossings are currently available only at the Grand River and Beck Road signal, the signal at the main entrance to the Suburban Showplace, and the Taft Road signal. The options for such crossing should be further studied as a joint effort between RCOC and the City. The Design Guide provides samples of elevated pedestrian bridges and street-level crossing with a raised median as possible design options to consider, however those are not the only alternatives. Traffic calming measures may also be recommended.

City West is meant to have sidewalks and pathways throughout the district, providing connections to the City's non-motorized network beyond the district. The recently expanded Ascension Providence campus trail network provides miles of non-motorized opportunities, and connects to the ITC Trail. Bosco Fields park is accessible via Beck Road less than 1 mile away. The planned project to expand Beck Road between 11 Mile and Grand River includes the completion of missing sidewalk segments on the east side of Beck Road. Taft Road already includes on-road bike lanes as well as an 8-foot pathway (except for a small segment near the intersection of Taft and 10 Mile where it drops to 5 feet) on the west side of the road from Grand River to south of 9 Mile. This facility connects the City West area to several Novi schools, as well as the library and Civic Center campus. Pathways along Grand River also provide a direct connection to the Town Center area within a 1-2 mile distance.

ORDINANCE FEEDBACK

Staff has worked with the Implementation Committee over the past year, and their feedback has been instrumental in getting the draft ordinance to this point. Last year, the committee recommended a design document to offer guidance to landowners, developers, and decision makers to illustrate the conceptual vision for the district. Based on that input, staff has created the City West Design Guide to serve as a companion document to the Zoning Ordinance text, which is meant to further explain and visualize the design standards. It includes background information on the area, inspiration images from around Novi and other communities, and 3D models to visually show certain requirements or suggestions. The intent is that both documents would be used in conjunction to guide development within the district. Each 3D model shown in the Design Guide is conceptual only; actual projects would need to be based on specific on-the-ground information not currently available to staff.

Staff has also engaged with property owners and relevant stakeholders through mail notifications, in-person meetings with developers, tenants, and landowners, and by providing a feedback form on the City website. In early November 2022, staff sent letters to property owners and current tenants with a link to a City web page with a feedback form in addition to access to the draft Ordinance text, draft Design Guide, and a map of the

district. Emails were sent out to known developers that had expressed an interest in properties within the area in the last several years with the same information. Staff has met with property owners and tenants to answer questions, provide clarifications and receive feedback.

All the responses from landowners within the district have showed positive support for the amendment to the Ordinance. Some property owners were concerned over how the change would affect a current use that would become nonconforming, but that concern was eased after explaining how nonconforming uses or structures would be permitted to continue, and even expand in a limited capacity. Some current nonconforming uses would actually become more conforming under the proposed standards, such as restaurant uses in the I-1 district.

Those spearheading new developments in the area also provided feedback on the Ordinance text. In response, we added lodge halls/fraternal organizations as a Special Land Use, as well as religious organizations/churches as a similar use. Another property owner met with staff to discuss plans to build a mixed-use hotel development on the south side of Grand River. Comments from him and his team suggested an increase in the allowable height for mixed use developments in the south area in addition to a minor change related to shared access drives.

A distinction remains between potential development north and south of Grand River Avenue, but only in density and height. There is no current distinction between north and south areas when it comes to uses, although townhouse-style residential buildings are more appropriate in the southern area where densities are lower.

A commercial broker representing the properties at the southeast corner of Beck and Grand River provided additional feedback on the text amendment and design guide. He noted the need for some clarification in the text to ensure that multifamily could be built as a stand-alone use – if there is proven connectivity. He also noted that the desired look may not be achieved with certain restrictions – namely, the minimum 5-acre parcel size and restrictions on residential density under the Mixed-Use Development option.

Staff also met with representatives from the Asbury Park Homeowners Association, who shared their concerns about noise, visibility, drainage, and property values. Issues of traffic and school impacts were also raised. Staff discussed the existing City regulations related to stormwater management, wetlands and woodland protection ordinance, which all projects would be subject to, and how the new ordinance was designed to minimize impacts to the existing neighborhoods through use and height restrictions, setbacks, and buffer requirements.

Although some adjustments were made to the draft text amendment based on the feedback received, other items were left as standards that could be determined on a case-by-case basis in the MDO process -- this is where some conditions or deviations can be up to Planning Commission and City Council's discretion to allow some flexibility to accommodate site-specific circumstances if it makes sense within the context of the development, neighboring uses, and the overall intent of the district.

DRAFT ORDINANCE CHANGES

Since the introduction to the proposed text amendment on April 5th, staff has continued to work on the draft ordinance. We included new landscaping standards within the Mixed-Use Development option to recognize that the more compact development style would need more urban-style landscaping. These landscaping standards include changes such as fewer trees per dwelling unit, allowing for movable planters to satisfy some landscaping requirements, and ensuring that street trees on internal drives are planted with enough room to ensure tree health. The height limits have been clarified throughout, and buildings within 200 feet of single-family residential districts are now proposed to be limited to 40 feet and will not be eligible for bonus height increases. Staff has added a requirement that applicants include a contextual plan sheet with general layout and uses within 300 feet of their property lines to illustrate a proposed project in context with its surroundings. Planning Commission will need to consider Special Land Use standards, along with other items, in making its recommendation to City Council.

Parking requirements for residential uses, based on number of bedrooms, have been included, which are consistent with the requirements in Town Center. Sidewalk requirements were also expanded on. An item was added to the approval standards to require applicants to demonstrate that the proposed layout of the site is designed to minimize the negative impact on existing natural features, including woodland and wetland areas. Additional changes included removing repetitive information, reorganization, fixing typos and renumbering for a consistent format.

NEXT STEPS

The Planning Commission is asked to hold the public hearing on the proposed text amendments and zoning map amendment, and consider making a recommendation to the City Council on these matters. The attached staff version of the proposed amendment is subject to review and changes by the City staff and/or the City Attorney's Office, and the Planning Commission and City Council, as it moves forward. Please contact Lindsay Bell 248.347.0484 or lbell@cityofnovi.org with any questions or concerns.

PROPOSED TEXT AMENDMENT 18.296 - DRAFT

STATE OF MICHIGAN
COUNTY OF OAKLAND
CITY OF NOVI

ORDINANCE NO. 18.296

AN ORDINANCE TO AMEND THE CITY OF NOVI CODE OF ORDINANCES, ORDINANCE 14-271, THE CITY OF NOVI ZONING ORDINANCE, AS AMENDED, AT ARTICLE 3.0 ZONING DISTRICTS, ADD SECTION 3.1.30 TO ESTABLISH THE CW – CITY WEST DISTRICT, SECTION 3.33 CITY WEST REQUIRED CONDITIONS, SECTION 3.34 CITY WEST MIXED USE DEVELOPMENT OPTION, RENUMBERING GENERAL EXCEPTIONS TO SECTION 3.35, AND TO AMEND ARTICLE 4.0 USE STANDARDS, SECTION 4.12 GROUP DAY CARE HOMES, DAY CARE CENTERS AND ADULT DAY CARE CENTERS, SECTION 4.24 DRY CLEANING ESTABLISHMENTS, SECTION 4.27 RETAIL BUSINESS OR SERVICE ESTABLISHMENTS, SECTION 4.28 HOTELS AND MOTELS, SECTION 4.35 MICROBREWERIES AND BREWPUBS, SECTION 4.62 INSTRUCTIONAL CENTERS, AND SECTION 4.81 FINANCIAL INSTITUTIONS. ALL BEING DONE TO CREATE AND ADD REQUIREMENTS FOR THE CITY WEST (CW) DISTRICT. AMEND ARTICLE 2, SECTION 2.2 DEFINITIONS, TO ADD A DEFINITION FOR WORKFORCE HOUSING. AMEND ARTICLE 3.0 ZONING DISTRICTS, SECTION 3.1.15 EXO EXPOSITION OVERLAY DISTRICT AND 3.25 EXO OVERLAY DISTRICT REQUIRED CONDITIONS, TO CHANGE THE UNDERLYING ZONING OF THE EXO OVERLAY DISTRICT TO CITY WEST.

THE CITY OF NOVI ORDAINS:

Part I.

That the City of Novi Zoning Ordinance, as amended, Article 3.0, Zoning Districts, Section 3.1 Districts Established, is hereby amended to add a new district as number 30 CW City West District.

Part II.

That the City of Novi Zoning Ordinance, as amended, Article 3.0, Zoning Districts, Section 3.1 Districts Established, is hereby amended to add section 3.1.30 as follows:

3.1.30 CW City West District

A. Intent and General Provisions

1. Description of the District. The City West district permits a mix of uses, including amusement and entertainment uses, office, commercial, and multiple-family housing, that are appropriate within the context of the City West area. Key design concepts include multiple-story buildings that frame the internal street network, a corridor of buildings massed along Grand River Avenue, shared access and parking, a mix of uses including commercial and multi-family residential, and public gathering spaces. Buildings should have high-quality materials and allow for changing uses over time.

Development will be pedestrian-oriented, with a high degree of transparency on the ground level and functional, attractive outdoor amenities such as plazas and pocket parks with seating and lighting. Parking is placed to the side or rear of main buildings, preferably in parking structures, driveways are consolidated from the major thoroughfare, and on-street parking is permitted on secondary streets.

2. Intent. The intent of the City West district is to encourage high-quality and distinctive development that will complement and support nearby areas. The district will reflect an inviting and vibrant identity for this area of the City's Grand River Corridor while implementing land use planning and development techniques that result in a pleasing, compatible, visual presence. Assembly of small lots and long, narrow lots into larger development parcels is anticipated and encouraged to provide coordinated development with consolidated access points. Minimum lot and frontage thresholds are established for the mixed-use development option to further this objective. Shared parking is strongly encouraged, and vehicular and pedestrian connections between sites is expected.

The district allows a wide range of principal uses. In conjunction with different height, area, and bulk standards, this should increase development and re-development opportunities that will look demonstratively different than existing uses.

Additionally, some of the objectives of the district are intended to be accomplished or facilitated by the establishment of an optional development approval process, the Mixed-Use Development Option (MDO). Under this optional form of development:

- Mixing of uses is permitted, either vertically or horizontally.
- Building heights are greater than most traditional commercial districts in the city.
- Floor area ratios and densities are increased to allow economies of scale.
- Required setbacks are reduced to encourage a development pattern that frames the primary street frontage.

Under the MDO concept, a wider variety of retail, commercial, office, and residential uses and open spaces are permitted, to further the creation of a dynamic mix of compatible uses. As a trade-off for this greater flexibility, regulations regarding streetscape and landscape design, provisions for parking facilities, architectural and façade design, multi-family residential dwelling units, and setback standards are incorporated into the district, resulting in a mix of some form-based standards and more conventional guidance. See the City West Design Guide for examples of the development vision and guidelines for projects in this district. The MDO, if successfully implemented, should:

- Encourage the use of land in accordance with its character and surroundings;
- Conserve natural resources and natural features;
- Encourage innovation and creativity in land use planning;
- Enhance pedestrian and cycling activity within the district and to surrounding destinations;
- Provide enhanced housing, cultural, and recreational opportunities for the people of the City; and

- Bring about a greater compatibility of design and use between and among neighboring properties.

B. Principal Permitted Uses –

- i. Offices, including professional, medical (including labs and clinics)
- ii. Municipal uses, such as post offices and similar governmental office buildings
- iii. Day care centers (4.12.2)
- iv. Financial institutions (4.81)
- v. Retail business or service establishments, including restaurants (4.27)
- vi. Business schools and colleges or private schools operated for profit (4.27)
- vii. Instructional Centers (4.62)
- viii. Outdoor theaters, plazas, parks, public gathering places, farmers markets and like public facilities
- ix. Art galleries, museums, and non-profit community centers
- x. Personal service establishments
- xi. Private recreational facilities, indoor or outdoor
- xii. Publicly owned and operated parks, pathways, and recreational facilities
- xiii. Transit station
- xiv. Brewpubs and Microbreweries (4.35)
- xv. Outdoor restaurants (4.84)
- xvi. Principal uses similar to those listed above, as determined by the Planning Commission
- xvii. Off-street parking lots and structures (not to include vehicle storage)
- xviii. Accessory structures and uses customarily incidental to the above permitted uses, except drive-through windows (4.19)

C. Mixed-Use Development Option Permitted Uses –

- i. Any of the Principal Permitted Uses above
- ii. Multiple-family residential
- iii. Live/work units
- iv. Hotels (4.28.1)
- v. Business establishments which perform services on the premises
- vi. Health and fitness clubs, public or private
- vii. Dry Cleaning Establishments or Pick Up Stations (4.24)
- viii. Other uses similar to the above uses subject to conditions noted
- ix. Accessory structures and uses customarily incidental to the above permitted uses (4.19)

D. Special Land Uses

- i. Amusement and entertainment uses, including theaters, athletic and performing arts venues

- ii. Private clubs, organizations, cultural facilities, and lodge halls
- iii. Places of worship
- iv. Drive throughs, as an accessory to a permitted use (Sec. 5.3.11)

E. Development Standards

Lot size, Lot Coverage, Lot width:

See section 3.6.2.D

Open Space:

Minimum Gross Open Space: 15% (see Section 3.33.4.1.F)

Minimum Usable Open Space: 150 sq ft per dwelling unit

Building Setbacks

Minimum front yard setback: See Section 3.33.1.E

Minimum rear yard setback: See Section 3.33.1.E

Minimum side yard setback: See Section 3.33.1.E

Building Height

Minimum building height: 2 stories or 20 feet

Maximum building height: 3 stories

Parking Lot Setbacks

Front yard setback: Front yard parking is not permitted, unless it is on-street, except as otherwise provided. See Section 3.33.1.E

Rear yard setback: 10 feet; If adjacent to existing residential zoning district a minimum of 50 feet is required

Side yard setback: 10 feet; If adjacent to existing residential zoning district a minimum of 50 feet is required

3.6.2 Applicability of Notes to District Standards

- o City West Districts: D, M,

Part III.

That the City of Novi Zoning Ordinance, as amended, Article 3, Zoning Districts, is amended to add a new Section 3.33, City West Required Conditions, as follows in its entirety:

3.33 City West District REQUIRED CONDITIONS

1. The following standards shall apply to all uses permitted in the district except as indicated otherwise in this ordinance, or where different or additional standards are either authorized or required for developments utilizing the Mixed-Use Development Option (MDO) in Section 3.34.

- A. Site Plans. Site plans shall be submitted for preliminary approval by the Planning Commission, in accordance with the provisions of this ordinance and other applicable ordinances in Section 6, unless the proposed plan qualifies for administrative review per Section 6.1.C.
- B. Nonconforming Uses and Buildings: A use or structure that is lawfully operating/existing as of the date this Section takes effect shall be considered a legal non-conforming use, so long as it remains otherwise lawful, subject to the following:
 - a. Such uses may be enlarged or increased by up to 10 percent of the existing total floor area, provided the resulting total floor area does not exceed the maximum floor area ratio of 0.275 as described below.
 - b. Any changes to the building or site at these locations that require site plan approval as described in Section 6.1 shall also require Planning Commission approval under the requirements of the Special Land Use permit approval provisions of that Section.
- C. Maximum floor area ratio (FAR): The maximum FAR (ratio of gross square feet of building area to gross land area of site, less prescribed right-of-way) shall be 0.275. Where an addition is proposed to an existing structure, the total resultant structure or combination of structures on-site shall be used in the calculations.
- D. Building and Parking Setbacks: Building and parking setback requirements, except as otherwise specified herein, shall be as noted in the table below, as measured from the future Right-of-Way line. These setback requirements may be reduced or increased by the approving body where strict adherence would serve no good purpose or where the overall intent of the district would be better served by allowing a lesser or greater setback, provided that:

- i. An increase or reduction of a setback will not impair the health, safety or general welfare of the city as related to the use of the premises or adjacent premise,
- ii. The increase or reduction of the setback along a common parcel line between two premises would result in a more desirable relationship between a proposed building and an existing building,
- iii. The increase or reduction of the setback would create a more desirable viewshed along the corridor, and
- iv. The adherence to the required setback would result in the establishment of non-usable land area that could create maintenance problems.

3.33.1.E. Setback Requirements for CW Districts		
Arterials (Grand River, Beck)	Minimum (feet)	Maximum (feet)
Building - Front	20	65
Building - Exterior side ¹	20	65
Parking ⁴	20	None
Nonresidential collectors and local streets	Minimum (feet)	Maximum (feet)
Building – Front	10	20
Building - Exterior Side ¹	0	none
Parking (rear and side yard) ⁴	10	none
Private Drives, Aisles & Shared Access Drives	Minimum (feet)	Maximum (feet)
Building – Front	10	none
Building - Exterior Side ¹	10	none
Parking (rear and side yard) ⁴	10	none
I-96 Highway ROW	Minimum (feet)	Maximum (feet)
Building ¹	75	none
Parking ⁴	30	none
Adjacent to Single Family	Minimum (feet)	Maximum (feet)
Building – Non-residential use	100	none
Building - Residential use	2 feet per each foot of building height	none
Parking (rear and side yard) ⁴	50 feet	none
Notes to Table		

1. Rear and interior side yard building setbacks are not prescribed, except as noted or as necessary to meet landscaping or other requirements, or where the Planning Commission or City Council determines buildings or parking require additional distance for safety, protection of natural features, or to enhance compatibility.
2. Distance Between Buildings: Buildings with a front-to-front relationship shall have a minimum separation of forty (40) feet. Buildings with a front-to-rear or front-to-side relationship shall have a minimum separation of thirty (30) feet. All other buildings shall have a minimum separation of twenty (20) feet, or thirty (30) feet for buildings sixty (60) feet or more in height, unless otherwise provided for in these district regulations.
3. The front yard setback shall be increased at intersections where necessary to obtain the appropriate clear vision area. Clear vision area shall comply with standards set forth in Section 5.9, Corner Clearance, or any more restrictive standards adopted by the Road Commission for Oakland County (RCOC).
4. Parking Setbacks.
 - a. Front yard off-street parking is generally not permitted as it detracts from the intent to create a pedestrian-oriented district. If parking in the front yard is determined to be essential to the site layout and uses, it shall be limited to one bay of parking and observe a minimum of a 10-foot setback with greenbelt. Surface parking lots shall be screened from all major thoroughfares by: (1) a two and one-half (2.5) foot ornamental brick or stone wall with intermittent landscaped breaks; or (2) semi-transparent screening such as brick pilaster with metal decorative fence in order to maintain attractive streetscapes as approved by the City's Landscape Architect. Landscaping, 12 - 36 inches in height, along the street side of walls or fences shall be provided.
 - b. Side yard parking adjacent to a front yard shall be setback from the front façade of the building by a minimum of five (5) feet. See illustration in Section 3.11.6.A. Screening as described above is required.
 - c. Off-street parking areas may be located within the exterior side yard along arterial roads if the primary building is oriented to front on a non-residential collector or local street. If parking is located in a side yard, the minimum setback of at least twenty (20) feet is required from the right-of-way line of any major thoroughfare, presently existing or as planned by the Road Commission for Oakland County or the City of Novi Master Plan.
 - d. Parking may front on shared private access road easements and similar private internal streets.
 - e. Parking adjacent to a single-family residential district shall have a minimum setback of 50 feet from that district. Off-street parking areas shall be screened with a 5-foot wall or landscaped berm.

- F. Height. Buildings shall be a minimum of two stories (or equivalent height with quality architectural design) and a maximum of three stories, except as otherwise provided under the MDO. Uncovered roof seating areas for restaurant uses may be permitted by the Planning Commission upon a demonstration by the applicant that such seating would not jeopardize public safety and/or privacy of adjoining uses and would not result in any other adverse consequences to the surrounding area, and particularly to residential uses.
- G. Pedestrian Development Amenities. Proposed developments, through innovative planning, design, and architecture, shall create a significant pedestrian orientation in keeping with the intent and purpose of the district and shall take into consideration the City West Design Guide recommendations.
- H. Sidewalks and Bicycle Facilities. Sidewalks with a minimum width of 6 feet are required abutting any street or internal road, and shall be placed at least 5 feet from the curb where street trees are required. Along major thoroughfares sidewalks shall be 8 to 10 feet wide as shown in the Active Mobility Plan. Sidewalks are also required between vehicular parking areas and building facades with pedestrian entrances. There shall be a system of pedestrian connections within and between all developments and between all buildings within a single development to enable pedestrians to safely access buildings and adjacent developments. Bicycle parking shall meet or exceed the standards set forth in Section 5.16, and bike paths shall be provided in accordance with the Active Mobility Plan.
- I. Open Space. A minimum of fifteen (15) percent of the gross site area of a commercial or mixed-use development shall be devoted to publicly accessible, permanently landscaped usable open spaces and pedestrian plaza areas (such as internal walkways, linear or pocket parks, plazas, or other space for gathering accessible to occupants and invitees). Small strips of landscaped area, landscaped end islands in parking lots, and similar areas that are less than twenty (20) feet wide shall not be counted toward the required open space on the site.

Up to 50 percent of the required open space may consist of areas that are regulated by City of Novi woodland and/or wetland ordinances if they are permanently protected by conservation easements. Wherever open spaces shall be created or preserved, they shall be physically connected, when possible, to adjacent open space areas.

Usable open spaces required for multi-family development units may count toward the 15 percent requirement if the spaces are publicly accessible (e.g., not private patios, roof decks, balconies, and clubhouses) and part of an integrated development plan. All landscaped open spaces and pedestrian plaza areas shall be maintained by the owners of the development it serves.

- J. Facades. All sites in the City West districts are subject to Region 1 standards of Section 5.15. In addition, all exterior walls of any main or accessory building shall be composed of the same or complementary architectural building facade materials as others on the site. Exterior building facades shall be primarily of brick or stone, which may be augmented by materials complementary to brick or stone. When renovations, alterations, or additions are made to an existing building within the City West district, the exterior building facades of the entire building shall be brought into compliance with this subsection.

Architectural design and facade material are to be complementary to existing or proposed buildings within the site and the surrounding area. It is not intended that contrasts in architectural design and use of facade materials is to be discouraged, but care shall be taken so that any such contrasts will not be so out of character with existing building designs and facade materials so as to create an adverse effect on the stability and value of the surrounding area.

- K. Parking, Loading, Landscaping, Lighting, etc. All sites must comply with Article 5, Site Standards, regarding all applicable requirements, including off-street parking, signs, landscaping, exterior lighting, and obscuring screens where abutting a single family district as those standards relate to uses permitted in the City West district, except as otherwise stated. Notwithstanding the above, loading and unloading may be in a side or rear yard, and shall be located as to be inconspicuous and properly screened. The approving body may modify the size requirement for a loading area, an alternate location, or approve a shared loading area between buildings, when the applicant provides ample justification that the uses proposed will be adequately served.

In those areas where on-street parking is permitted, off-street parking requirements may be reduced by the number of on-street parking spaces adjacent to a use. Parking requirements may be reduced when the approving body determines that given parking areas serve dual functions by providing parking capacity for separate uses which have peak parking demand periods which do not overlap. The applicant shall submit a Shared Parking Study based on requirements set forth in Section 5.2.7 to provide justification for the request. Applicant shall also provide information on cross-access and maintenance agreements.

In those instances when a parking structure, underground parking or municipal parking lot is constructed pursuant to a special assessment district, a parking exempt district shall be created for that area served by the improvement and assessed for its cost.

- L. Vehicular Access. Vehicular access to a development site shall be designed to provide safe and efficient distribution of traffic to and from the site and should

form a logical street network connecting to adjacent parcels in the district where appropriate. Access design that results in an undue intensification of traffic congestion shall be prohibited. Multiple curb cuts onto major arterials is discouraged.

M. Street and Roadway Rights-of-Way. Nonresidential collector and local streets within the City West district shall provide rights-of-way and road cross sections consistent with the City's Design and Construction Standards. Additional rights-of-way area shall be provided where boulevards, squares or traffic circles are created.

N. Storm Water Detention. The use of underground and shared storm water facilities meeting all relevant standards should be used to the extent feasible to achieve permitted densities while protecting existing natural resources.

Part IV.

That the City of Novi Zoning Ordinance, as amended, Article 3, Zoning Districts, is amended to add a new Section 3.34, Mixed Use Development Option for the City West District, as follows in its entirety:

3.34 Mixed Use Development Option (MDO) for the City West District

1. Intent and General Application. The MDO expands the number and kind of uses permitted in the district. In addition to permitting quality residential development and facilitating mixed-use developments, including multiple-family residential, office, and commercial uses, this optional form of development allows for increased building height—up to 110 ft—and increased floor area ratios (FAR), as well as the opportunity to deviate from height, area, and bulk standards provided the deviations benefit the development and are considered in context with the surrounding area.

The granting of a Mixed-Use Development Option application requires review and recommendation by the Planning Commission and approval of the City Council, under the process contemplated in MCL 125.3503. As part of the review and appeal process, and in light of the potential additional uses and flexible building standards, the development may be subject to certain conditions, and will have to demonstrate conformance with the Design Guide adopted by the City as part of this ordinance.

For purposes of this Section 3.34, the “City West North” area shall be that part of the district north of Grand River Avenue, and the “City West South” shall be that part of the District south of Grand River Avenue.

2. Uses permitted subject to approval of a Mixed-Use Development Option

A. For developments utilizing the Mixed-Use Development Option (MDO) the uses listed under Section 3.1.30.B, C, and D will be permitted, with a minimum combination of two distinct use classifications. Multiple-family residential dwellings,

including senior, age-qualified, or independent housing, may be proposed as a stand-alone use.

- B. MDO Projects with a single use may be considered for density reserved for mixed-use projects if there is a clearly designed relationship between complementary uses, including direct pedestrian connections and building entrances located within 300 feet measured along the pedestrian route.

3. Eligibility Criteria

- A. MDO projects shall require the applicant to demonstrate that each particular use (or single use, as applicable), as well as the quantity and location of such use(s), would result in a reasonable and mutually supportive mix of uses on the site, and a compatibility of uses in harmony with the surrounding area and other nearby areas of the City, as intended in this Article.
- B. Minimum acreage for a project is five (5) acres, unless varied by City Council approval with a demonstration by the applicant that the proposed development on less land meets the standards of Section 3.34, and subsections 3.33.1 and 3.33.3, and that the proposed development exemplifies the intent of this Article as stated in Section 3.1.30.A and the Design Guide.
- C. Minimum public road frontage is three hundred (300) feet unless varied by City Council.

4. General Approval Standards

- A. As part of the application and review for site plan approval, the applicant for the MDO must demonstrate the following:
 - i. The project will result in a recognizable and substantial benefit to the ultimate users of the project and to the community, where such benefit would otherwise be unfeasible or unlikely to be achieved by a traditional development.
 - ii. Relative to a development otherwise permissible as a Principal Permitted Use under [Section 3.1.30.B](#), the proposed type and density of development shall not result in an unreasonable increase in the use of public services, facilities and utilities, and shall not place an unreasonable burden upon the subject and/or surrounding land and/or property owners and occupants.
 - iii. The layout of the site is designed to minimize the negative impact on existing natural features, including woodlands and wetland areas.
 - iv. Based upon the proposed uses, layout and design of the overall project, the proposed building facade treatment, the proposed landscaping treatment and the proposed signage, the MDO project will result in a material enhancement to the area of the City in which it is situated.
 - v. The proposed development will not have a materially adverse impact upon the Master Plan for Land Use of the City and will be consistent with the intent and spirit of this Section.
 - vi. Each proposed use in the development, as well as the size and location of such use, will result in and contribute to a reasonable and mutually supportive mix of uses on the site, and/or a compatibility of uses in harmony with the surrounding area and other nearby areas of the City.

- vii. The land area proposed for development represents a cohesive and logical consolidation of parcels to allow a coordinated project to be brought forward.
- viii. The proposed development will be under single ownership and/or control such that there is a single person or entity having responsibility for completing the project in conformity with this Ordinance. This provision shall not prohibit a transfer of ownership and/or control, upon completion of one or more phases or upon 1) due notice to the City Clerk, 2) appropriate and customary approval by other City officials and 3) financial guarantees for completion of improvements.

5. Project Design Standards. The following project design standards shall apply to MDO projects:

- A. General Design Standards. The following design standards shall apply to all projects:
 - i. Permitted non-residential uses may be allowed in combination with other permitted non-residential or residential uses, based upon a layout and integrated plan approved as part of the site plan.
 - ii. The design standards for non-residential uses shall be based upon the regulations in this Ordinance applicable to the corresponding uses, provided that modified design standards may be approved by the City Council based upon a demonstration by the applicant of the following, and the findings of Section 3.34.7.A.i.d:
 - a. The proposed uses will complement and support the intent of the City West district, the convention center and other established uses within this area of the City.
 - b. The project has shown that design coordination and connection with adjacent property, developed or not, has been accomplished;
 - c. An attractive street-level environment has been achieved that focuses on the pedestrian experience and provides public space amenities;
 - d. Vehicular safety provisions and controls have been applied particularly with regard to access to major thoroughfares; and
 - e. Aesthetic quality is improved in terms of design, exterior materials and landscaping, including internal compatibility within the development as well as its relationship to surrounding properties to accommodate future development.
 - iii. There is underground installation of utilities, including electricity and telecommunications facilities, as found necessary or appropriate by the approving body.
 - iv. In order to provide efficient circulation and reduce driveways and curb cuts along Grand River Avenue, all development sites fronting Grand River Avenue shall be constructed to maximize traffic safety. Toward this end, to the extent feasible internal access drive networks shall be established and utilized as local streets. The shared access drives shall be privately owned and maintained, have a cross-section meeting the City's local street standards, and shall be governed by a cross-access agreement that

provides for public access at all times and shall be recorded with the Oakland County Register of Deeds after review and approval by the City Attorney.

The shared access drive shall be a minimum of twenty-eight (28) feet wide (from back of curb) and shall be placed within a thirty-six-foot (36) (minimum) private easement. Parallel parking may be permitted along said shared access drive provided an access aisle of 26 feet is maintained.

The City Council may waive the requirement for a shared access drive where it is not feasible to extend it to another property due to 1) environmental limitations, 2) incompatible adjacent development, 3) shallow lots, or 4) other unique site features.

- v. The City Council shall resolve ambiguities in the interpretation of applicable regulations using the Zoning Ordinance, Master Plan, the Design Guide, and the intent of this Article and other City standards or policies as a guide.
- B. Bonus height. Buildings in an MDO project may exceed the height stated in the Development Standards, except those buildings located within 200 feet of a single-family district, subject to the following:
- i. Bonus height may be granted under the following circumstances:
 - a. If underground or structured parking is provided, the maximum height of the building may be increased up to 2 additional floors (1 story per 125 spaces provided);
 - b. If dedicated Open Space or preservation of natural areas is provided in excess of 25 percent of the total site area, the maximum height of the building may be increased an additional story.
 - c. The provision and use of energy and water efficient design, water conservation, reuse and preservation of resources, and sustainable lifestyle solutions such as electric vehicle charging or integration of solar arrays. Applicants shall provide a narrative explaining how sustainability elements will be incorporated and quantify the impacts of those strategies. For qualifying for Gold or Platinum LEED (Leadership in Energy and Environmental Design) certification, or equivalent for green building strategies, the maximum height of the building may be increased an additional floor (up to 1 story).
 - d. Providing 15 percent or more residential units targeted for workforce housing as defined in Section 2 (1 story bonus).
 - ii. Building height, including bonuses earned, shall not exceed one hundred ten (110) feet or 8 stories, whichever is less, in the City West North area or 70 feet or 5 stories, whichever is less, in the City West South area.

- iii. Buildings utilizing this subsection shall be designed to minimize their impact on surrounding existing uses and roadways, including, but not limited to, building design elements such as variation in building materials, mitigation of exterior and interior building lighting, and utilization of building relief strategies (including step-backs of higher stories).
 - iv. For all buildings utilizing bonus height strategies above, the City Council, following a recommendation by the Planning Commission, shall make a finding that the additional height will complement and be compatible with the vision for the district with respect to the size, height, area, and configuration of adjacent or surrounding parcels and structures and any other relevant characteristics and interest. The City Council shall determine whether the architectural design of the buildings provides adequate building relief to minimize the mass and height of the building and will not have a negative impact on the goal of creating a cohesive, walkable district.
6. Required Site Development Conditions for MDO projects
- A. The required conditions listed within Section 3.33 must be met except as otherwise permitted within this Section 3.34
 - B. The following standards are required for residential-only buildings, either as a component in a mixed-use or a single-use development:
 - i. All residential structures shall have a minimum of two (2) stories or equivalent height.
 - ii. The minimum distance between townhouse buildings shall be twenty (20) feet. No more than eight (8) attached townhouse units in a single building.
 - iii. The minimum distance between multifamily buildings shall be one-half (1/2) the height of the taller building.
 - iv. The length/depth of a building shall not exceed four (4) times the height of the building.
 - v. Private community swimming pools and similar private amenities shall not require additional parking spaces except for barrier free spaces.
 - vi. Off-street parking shall not be placed within ten (10) feet of any wall of a dwelling structure. Units that have garages may be permitted parking on garage aprons if parking space dimensions are met without encroaching on sidewalks.
 - vii. Landscaping for residential-only buildings (See Section 5.5 for definitions):
 - a. Multi-family Unit Trees: 1.5 trees per first floor unit
 - b. Multi-family Unit Shrubs: 2 shrubs per first floor unit
 - c. Interior Street Trees: 1 canopy tree per 50 linear feet (less driveway widths)
 - viii. Parking requirements:
 - a. Studio and 1-bedroom units: 1 space per unit
 - b. 2+ bedroom units: 2 spaces per unit

- C. The following additional site development conditions shall apply to all MDO projects:
 - i. Commercial and office uses may occupy any number of total floors within a building used for residential uses. No commercial or office use shall be located on the same floor as residential use, unless it is considered a live work unit, and no floor may be used for commercial or office purposes which is located above a floor used for residential purposes.
 - ii. All trash receptacles and trash collection areas shall be screened from view and shall not be placed within ten (10) feet of any wall of a dwelling structure which contains openings involving living areas.
 - iii. HVAC units that are not placed on the roof of a structure must be located away from openings to dwellings, including windows, and must be screened from view.
 - iv. Buildings may be permitted to have parking on the ground level of the building. The parking inside the building must be aesthetically and effectively screened from view through architectural design, landscaping, or other means, from adjacent drives, walkways and buildings, and particularly from the street level view.

- D. The following densities/intensities shall be permitted based on mixed-use or single-use developments:

Table 3.34.6.D Development Density/Intensity – Based on Net Site Area			
	Residential Only	Mixed-Use Developments	Mixed-Use Developments
A. Maximum Density ¹		Single Use Building	Multi-Use Building
City West North	20 du/ac	25 du/ac	30 du/ac
City West South	10 du/ac	15 du/ac	20 du/ac
B. Floor Area Ratio ¹			
City West North		2.0 FAR	2.5 FAR
City West South		1.0 FAR	1.5 FAR
C. Maximum Building Height ²			
City West North	65 feet	65 feet	65 feet
City West South	45 feet	55 feet	55 feet
D. Minimum Building Height ²			
City West North	35 feet	40 feet	40 feet
City West South	25 feet	25 feet	35 feet

Notes to Table

1. For all development, density/intensity shall be calculated for the net site area of the development.
2. Any structure more than 100 feet but less than two hundred (200) feet of a one-family residential district shall be limited to a maximum of forty (40) feet or three stories. In all other locations building height may be exceeded with bonus height as described in subsection 3.34.5.B.
3. Buildings with a front-to-front relationship shall have a minimum separation of forty (40) feet. Buildings with a front-to-rear or front-to-side relationship shall have a minimum separation of thirty (30) feet. All other interior buildings shall have a minimum separation of twenty (20) feet, or thirty (30) feet for buildings sixty (60) feet or more in height.

E. Landscape Standards at Major Thoroughfares

- i. Greenbelt: A landscape planting buffer is required to improve the appearance from the right-of-way, including screening off-site parking and drive areas.
 - a. Greenbelt that is adjacent to on-site drive or parking shall be a minimum of 10-feet in width, with a brick wall 3 feet in height to provide screening, and small plantings on the street side. The wall should be placed 3 feet from the curb. Trees should be planted within 4-foot gaps between wall sections.
 - b. Greenbelt that is not adjacent to parking or drive shall be a minimum of 20 feet in width. No screening wall is required.
 - c. The following greenbelt landscaping is required: 1 canopy tree per 30 feet or 1.5 subcanopy trees per 30 feet (less drive width). Trees to be evenly spaced.

F. Landscape Requirements for Mixed-Use and Commercial Buildings are shown in Table 3.34.6.F below:

Location	Minimum Requirement
i. Interior Drives	
Street trees	1 canopy tree per 75 feet ¹ on both sides, spaced not closer than 25 feet
Minimum greenspace/tree	200 sf or silva cells (or comparable alternative) with 1,000 ft ³
ii. Foundation Landscaping	
Sidewalk 12 ft or wider	15% of building frontage, 2-3 feet depth
Sidewalk less than 12 ft	None

iii. Open Space	
Paved gathering areas	5% landscaped, including movable planters
Outdoor dining areas	60% of periphery landscaped with permanent planting beds or movable planters
Primarily unpaved areas	1 canopy tree + 1 subcanopy tree + 10 shrubs per 5,000 sf of area
Landscaping feature	1 per 10,000 sf of open space in the development
iv. Parking	
Surface Lots	200 sf & 1 canopy tree per 10 spaces, 15 space contiguous limit
Parking Structures	35% of building frontage (trellis or planted walls can meet requirement)
Notes to Table	
<ol style="list-style-type: none"> 1. Measured along linear foot of street/drive, less driveway widths. See Figure 5.5.3.B.ii 2. Buffers are not required between developments within the district, but may be desirable in some circumstances to enhance compatibility between different uses. 	

5. Review and approval process for all MDO projects

A. Preliminary Application and Review

- i. Pre-Application Submittal. Before formal submittal of a MDO plan, the applicant shall apply for and attend a pre-application meeting with the Plan Review Center, and other appropriate officials deemed necessary by the City Planner. Prior to the meeting, the applicant shall provide to the Plan Review Center a conceptual site plan and brief narrative of how the development meets or exceeds the standards of this ordinance and furthers the vision for the district. The Plan Review Center will provide initial comments on the plans.
- ii. Preliminary MDO Application.
 - a. An application to develop under the MDO shall be made to the Planning Commission for its review and recommendation to City Council. The application shall be filed concurrently with a fully documented Preliminary Site Plan. The Preliminary Site Plan shall be completed in accordance with the review requirements as set forth and regulated in the City's Site Plan and Development Manual and Section 6.1. The MDO plan shall include the information required in [subsection 3.34.7.C](#).
 - b. In those instances where it is necessary to request rezoning in order to effectuate a MDO plan, an application to rezone shall be made to the Planning Commission for its review and recommendation to the City Council. The application for rezoning shall be completed in accordance with the City's Site Plan & Development Manual.

- c. The Planning Commission may proceed simultaneously with review and recommendation on applications for rezoning, MDO plan and Preliminary Site Plan approval, along with any associated permit reviews.
- iii. Planning Commission Review. Upon receipt of an application for preliminary site plan, the application shall be reviewed by the Plan Review Center and referred to the Planning Commission for preliminary review. Following preliminary review, the Planning Commission shall set a public hearing date for review of the application. Notice of the public hearing shall be given and the hearing held as provided by the Zoning Enabling Act, at which a review shall be conducted. Following such review, the Planning Commission shall provide its report and recommendation to the City Council. In making its recommendation to the City Council, the Planning Commission shall consider:
 - (1) Consistency with the Master Plan;
 - (2) Innovative planning and design excellence;
 - (3) Relationship to adjacent land uses, design form and layout;
 - (4) Compliance with this Ordinance, including subsections 3.33 and 3.34;
 - (5) Adherence to the City West Design Guide;
 - (5) Benefits to the community such as publicly accessible parks and open areas, and public facilities.
 - (6) Pedestrian and vehicular safety provisions.
 - (7) Aesthetic beauty in terms of design, exterior materials, landscaping, and natural features, including internal compatibility within the development as well as its relationship to surrounding properties, and
 - (8) Provisions for the future users of the project.
 - (9) The standards of Section 6.1.2.C for Special Land Uses.
- iv. City Council Approval. The City Council shall review the Preliminary Site Plan with regard to the Planning Commission's recommendation and the review requirements and conditions set forth in Section 3.34.4.A. The City Council shall determine whether the proposed plan meets the conditions as set forth in Section 3.33 and 3.34, and the standards of [Section 6.1.2.C](#).
 - a. As part of its approval of the Preliminary Site Plan, the Council is authorized to impose conditions that are reasonably related to the purposes of this section and that will:
 - Insure public services and facilities affected by a proposed land use or activity will be capable of accommodating increased services and facility loads caused by the land use or activity,
 - Protect the natural environment and conserve natural resources and energy,
 - Insuring compatibility with adjacent uses of land, and
 - Promote the use of land in a socially and economically desirable manner.

All conditions imposed shall be made a part of the record of the approved Mixed Use Development Option.

- b. Deviations From Density, Area, Bulk, Yard, and Dimensional Requirements. As part of approval of a Preliminary Site Plan, the City Council shall be authorized to grant deviations from the strict terms of the zoning ordinance governing density, area, bulk, yard, and dimensional requirements applicable to the property; provided, however, that such authorization to grant deviations shall be conditioned upon the Council finding:
 - 1. That each zoning ordinance provision from which a deviation is sought would, if the deviation were not granted, prohibit an enhancement of the development that would be in the public interest;
 - 2. That approving the proposed deviation would be compatible with the existing and planned uses in the surrounding area;
 - 3. That the proposed deviation would not be detrimental to the natural features and resources of the affected property and surrounding area, or would enhance or preserve such natural features and resources;
 - 4. That the proposed deviation would not be injurious to the safety or convenience of vehicular or pedestrian traffic; and
 - 5. That the proposed deviation would not cause an adverse fiscal or financial impact on the City's ability to provide services and facilities to the property or to the public as a whole.

- c. In determining whether to grant any such deviation, the Council shall be authorized to attach reasonable conditions to the Site Plan that will:
 - 1. Insure that public services and facilities affected by a proposed land use or activity will be capable of accommodating increased services and facility loads caused by the land use or activity,
 - 2. Protect the natural environment and conserving natural resources and energy,
 - 3. Insure compatibility with adjacent use of land, and
 - 4. Promote the use of land in a socially and economically desirable manner.

All conditions imposed shall be made a part of the record of the approval of the Site Plan.

- B. Approved Preliminary Site Plan. Once the Preliminary Site Plan has been approved by City Council, no development shall take place therein nor use made of any part thereof except in accordance with the approved plan or in accordance with an approved amendment thereto. Once approval of the Preliminary Site Plan is granted, the applicant shall proceed with development of the Final Site Plan.

- C. Final Site Plan approval. A Final Site Plan shall be prepared and submitted by the applicant for approval. Review and approval shall be administrative unless the City Council requests that the Final Site Plan be submitted to it for review. The Final Site Plan shall:

- i. Conform with the Preliminary Site Plan and meet the conditions as set forth in the Council's approval.
 - ii. Contain all the requirements as set forth in Section 6.1 of this Ordinance and the requirements as set forth in the City's Site Plan and Development Manual.
 - iii. The dedication of public rights-of-way or planned public open spaces, where proposed on the site plan or as may be otherwise required, shall have been made.
 - iv. In residential use areas, any proffered open space has been irrevocably committed to be retained as open space for park, recreation and related uses, and that all such lands meet the requirements of the City.
 - v. Where applicable, road easements or rights-of-way have been provided.
- D. Site Plan Revisions. Revisions to an approved Preliminary Site Plan or Final Site Plan shall require re-submittal of plan revisions to the City for review. Such revisions shall be resubmitted to the Planning Commission and City Council for review and approval, except those revisions permitted to be reviewed administratively under Section 6.1.1.C. Revisions requiring Planning Commission and City Council review shall include physical changes to the exterior building wall façade materials or layout alterations that will significantly change the appearance of a building or site, as determined by the City Planner. The Planning Commission and City Council, in making a review of a revised site plan, shall find that any such revisions forwarded to them for review and approval, meet all the minimum requirements of this Section, including general intent.
- E. Phasing: Where a project is proposed for construction in phases, the planning and designing shall be such that, upon completion, each phase, considered together with other completed phases, shall be capable of standing on its own in terms of the presence of services, facilities, and open space, and shall contain the necessary components to insure protection of natural resources and the health, safety, and welfare of the users of the planned mixed use development and the residents and property in the surrounding area. Plans shall clearly illustrate what is to be included in each phase. The Council may require performance guarantees in accordance with Chapter 26.5 to ensure that permanent facades are constructed. In developments which include residential and non-residential uses, phasing shall be consistent with the following:
- i. At least thirty (30) percent of all proposed residential units and non-residential floor area shall be constructed no later than concurrent with the first phase of construction.
 - ii. At least an additional thirty (30) percent of all proposed residential units and twenty (20) percent of non-residential floor area shall be constructed no later than concurrent with any second phase construction; and

- iii. The balance of all proposed residential units shall be constructed no later than concurrent with any third phase of construction. For purposes of implementing the residential/non-residential phasing requirements set forth above, the percentages shall be reasonable approximations, as determined in the discretion of the City Council, and such percentages may be substantially modified should the City Council determine in its discretion that the applicant has presented adequate and effective assurances that both residential and non-residential component(s) of the development shall be completed within a specified period.

8. Plan Information

A. MDO Preliminary site plans shall include the following:

- i. All requirements listed in the City's Preliminary Site Plan Checklist
- ii. Statement of intent of proposed use(s) of land and any phasing of the project, including anticipated completion dates.
- iii. Evidence of market need for the use(s) and economic feasibility of the project.
- iv. Qualifications of applicant/developer including information of past projects, size, location, type etc.
- v. A Contextual plan sheet with the general layout and all uses within 300 feet of the property lines to demonstrate the proposed project in context with the surroundings.
- vi. Existing and proposed right-of-way width of all adjoining and internal roads, and layout of all internal roads and drives.
- vii. A traffic study, prepared in accordance with the City's Site Plan and Development Manual requirements, shall be submitted, unless such requirement is waived by the Plan Review Center during pre-application conference.
- viii. Proposed acceleration, deceleration, and passing lanes.
- ix. All parking areas and number of spaces by size and any requests for shared parking reductions. The City may permit the use of a flexible shared parking formula if a Shared Parking Study is submitted and accepted by the City Council, after review and recommendation by the Planning Commission and City's traffic consultant.
- x. The size and location of areas to be preserved as open and recreational space and intended programming of those spaces.
- xi. Density calculations, number and types of units (if applicable), and floor area per habitable space for each use proposed.
- xii. Fair representation of each type of use, square footage or acreage allocated to each use, locations of each principal structure and use in

the development, setbacks, typical layout and elevation for each type of use.

- xiii. Specification of each deviation from the applicable ordinance regulations which will be sought to be approved, and the safeguards, features and/or planning mechanisms proposed to achieve the objectives intended to be accomplished by the regulations from which a deviation is being sought.
- xiv. Community Impact Statement for all projects, including impact on City services, fiscal impact analysis addressing anticipated costs and revenues to City and school district, unless such requirement is waived by the Plan Review Center during pre-application conference.
- xv. If phasing is proposed, a plan showing what improvements will be constructed in each phase, with a description of the anticipated timing of construction.

B. Final site plans shall include the following:

- i. All requirements specified in the City's Final Site Plan Checklist and Application.
- ii. List all deviations from Ordinance standards granted by City Council and any conditions imposed.
- iii. If phasing is proposed, provide detailed plans that show what will be constructed/installed by the completion of each phase to verify that each phase, considered together with previous completed phases, shall be capable of meeting ordinance requirements on its own as described in Section 3.34.7.E, including utilities, parking, end of pavement, landscaping, amenities, lighting, signage, any mechanisms designed to reduce noise, and visual screening features. Required Engineering and Landscape Cost estimates shall also be broken down by phases.

9. Performance Guarantees. The applicant shall comply with the requirements for performance guarantees contained in Chapter 26.5 of the Code of Ordinances, as amended.

Part V.

That the City of Novi Zoning Ordinance, as amended, Article 3, Zoning Districts, is amended to renumber Section 3.33, General Exceptions to Section 3.35.

Part VI.

That the City of Novi Zoning Ordinance, as amended, Article 4, “Use Standards,” Section 4.12 “Group Day Care Homes, Day Care Centers, and Adult Day Care Centers” is amended to read as follows:

Section 4.12 Group Day Care Homes, Day Care Centers, and Adult Day Care Centers

1. [unchanged]
2. Day care centers and adult day care centers are a permitted use in the B-2, B-3, OST, TC, TC-1, CW districts and EXO Overlay district and a special land use in the OS-1, OSC, and PSLR districts, all subject to the following:
 - A. In the B-2, B-3, OST, OS-1, OSC, TC, TC-1, CW, PSLR districts and EXO Overlay district:
 - i. Outdoor recreation areas shall be provided, consisting of at least one -hundred fifty (150) square feet for each person cared for, with a minimum total area of three-thousand five hundred (3,500) square feet. All such outdoor recreation areas shall be fenced with self-closing gates. The recreation area may extend into an exterior side yard up to twenty-five (25) percent of the distance between the building facade and the property line.
 - ii. The hours of operation shall be limited to the period between 6 a.m. and 7 p.m. for those facilities abutting residential zoning districts.
 - iii. Facilities shall be located either within a permitted office, or commercial structure, or in a freestanding building on a site coordinated with surrounding development (i.e., traffic flow, parking access, drop off areas, architecture and relationship to other buildings).
 - iv. Screening and landscaping of outdoor recreation areas, recreation area fences and parking lots shall comply with Section 5.5.
 - v. Off-street parking shall comply with Section 5.2.12. and Section 5.3.
 - B. [unchanged]
3. [unchanged]

Part VII.

That the City of Novi Zoning Ordinance, as amended, Article 4, “Use Standards,” Section 4.24 “Dry Cleaning Establishments or Pick Up Stations,” is hereby amended to read as follows:

Section 4.24 Dry Cleaning Establishments or Pick Up Stations

In the B-1, B-2, B-3, CW and TC districts, dry cleaning establishments, or pick-up stations, dealing directly with the consumer are a permitted use. Central dry cleaning plants serving more than one retail outlet shall be prohibited.

Part VIII.

That the City of Novi Zoning Ordinance, as amended, Article 4, "Use Standards," Section 4.27, "Retail Business and Service Establishments," is hereby amended to read as follows:

4.27 Retail Business or Service Establishments

1. In the B-2, B-3, CW, TC and TC-1 districts, all retail business or service establishments are permitted as follows:
 - A. Any retail business whose principal activity is the sale of merchandise in an enclosed building.
 - B. Any service establishment of an office, showroom or workshop nature of a decorator, dressmaker, tailor, bridal shop, art gallery, interior designer or similar establishment that requires a retail adjunct.
 - C. Restaurants (sit down), banquet facilities or other places serving food or beverage, except those having the character of a drive-in or having a drive-through window.
 - D. Theaters, assembly halls, concert halls, museums or similar places of assembly when conducted completely within enclosed buildings.
 - E. Business schools and colleges or private schools operated for profit.
2. In the FS district, retail establishments to serve the needs of the highway travelers, including such facilities as, but not limited to, gift shops and restaurants, not including drive-ins are permitted uses.

Part IX.

That the City of Novi Zoning Ordinance, as amended, Article 4, "Use Standards," Section 4.28 "Hotels, Motels, and Transient Lodging Facilities," is hereby amended to read as follows:

Section 4.28 Hotels and Motels

1. In the B-2 and CW district, hotels and motels are a permitted use provided the site does not abut a residential district.
2. (Unchanged)
3. (Unchanged)
4. (Unchanged)
5. All hotels and motels in all districts, shall provide a minimum of 30 square feet of usable open space for each room subject to the following conditions:
 - A. Usable open space shall include an unobstructed portion of a site which has a minimum dimension of ten feet in any direction, is landscaped and developed for active and passive recreational use, and is conveniently located and accessible;

- B. The usable open space may include amenities such as pavers, benches and other landscape design elements with pervious surface materials. All such areas shall be for the benefit of the hotel patrons;
 - C. For TC, TC-1, CW, and PD-2 districts, the usable open space required per this section can be counted towards the minimum usable open space requirements for respective district standards;
 - D. The usable open space shall not be counted toward any minimum landscaping or other open space requirements of the Zoning Ordinance and required parking setback area;
6. (Unchanged)

Part X.

That the City of Novi Zoning Ordinance, as amended, Article 4, “Use Standards,” Section 4.35 “Microbreweries and Brewpubs,” is hereby amended to read as follows:

Section 4.35 Microbreweries and Brewpubs

Microbreweries and brewpubs are permitted uses in the B-3 and CW districts and special land uses in the RC, TC, and TC-1 districts subject to the following conditions:

- 1. Microbreweries and brewpubs
 - A. In the B-3, CW, RC, TC and TC-1 districts:
 - i-iv. (Unchanged)
 - B. (unchanged)
- 2. (unchanged)
- 3. (unchanged)

Part XI.

That the City of Novi Zoning Ordinance, as amended, Article 4, “Use Standards,” Section 4.62 “Instructional Centers,” is hereby amended to read as follows:

Section 4.62 Instructional Centers

In the CW and NCC districts, instructional centers, such as schools for dance, music, language, arts, or general education are a permitted use subject to the following:

- 1. The center must comply with all applicable state laws and licensing requirements.
- 2. All business, servicing or processing, except for off-street parking or loading, shall be conducted within a completely enclosed building.

In the OS-1 district, instructional centers are permitted uses provided that such facilities do not exceed two-thousand (2,000) square feet in size. However, when such uses exceed two-thousand (2,000) square feet, they shall be treated as special land uses subject to approval by the Planning Commission in accordance with the additional requirements of Section 6.1.2.C for special land uses, and subject to the public hearing requirements set forth and regulated in Section 6.2 of this Ordinance. All instructional centers shall comply with the following conditions:

1. All business, servicing or processing, except for off-street parking or loading, shall be conducted within a completely enclosed building.
2. Multiple tenants with square footage less than 2,000 square feet shall be allowed in a single building.

Part XII.

That the City of Novi Zoning Ordinance, as amended, Article 4, “Use Standards,” Section 4.81 “Financial Institutions,” is hereby amended to read as follows:

Section 4.81 Financial Institutions

In the CW, TC and TC-1 districts, financial institutions are a permitted use, provided that such institutions shall not have drive-thru teller or ATM facilities as the principal use of the premises.

Part XIII.

That the City of Novi Zoning Ordinance, as amended, Article 2, Definitions, Section 2.2. Definitions, is hereby amended to include the following definitions:

Section 2.2 Definitions

Workforce Housing: A residential unit for sale or for rent with combined annual rental costs or combined annual mortgage loan debt service, property taxes, and required insurance that do not exceed thirty percent (30%) of the gross annual income of a household earning between sixty percent (60%) and one hundred and forty percent (140%) of the area median income, as defined annually by the United States Department of Housing and Urban Development.

Part XIV.

That the City of Novi Zoning Ordinance, as amended, Article 3, Zoning Districts, Section 3.1.15 EXO Exposition Overlay District, is hereby amended to read as follows:

Section 3.1.15 EXO Exposition Overlay District

A. Intent

The EXO, Exposition Overlay district is designed to accommodate the development of a planned exposition, convention, and conference facility, including exhibit halls; display floor area; meeting rooms; conference rooms; eating and lodging facilities; banquet, concession, and catering uses; museums; theaters; recreational facilities; space for school and civic affairs; warehousing accessory to principal uses; and off-street parking and loading/unloading space, all for the purpose of supporting the growing high-tech business base within the city and regional demand for an exposition facility.

The EXO Exposition Overlay district is intended to encourage the development of an exposition, conference, and convention facility that is located near the City's Town Center (TC) City West (CW) district as shown on the Master Plan for Land Use, has visual exposure to the I-96 freeway (whether directly or in combination with other property), is accessible from a major thoroughfare, and has adequate site area. Taking into

consideration the need to limit the overall impact of the use, and the need to have sufficient population support for the use, in no event is it the intent to have the EXO Overlay District encompass more than 55 contiguous acres, or to have more than one EXO Overlay district until the population of the City exceeds 100,000. The EXO district is intended to be utilized exclusively in connection with the ~~OST Planned Office Service Technology district~~ CW City West district as the underlying zoning district. The EXO district is not intended to be adjacent to residential districts. To permit quality design and sound economic value for the City, any exposition, conference, and convention facility should have unified architectural and functional design and provide an overall master plan for all phases for the complete development. The district contemplates the existence of a principal exposition, conference, or convention facility as part of the development before, or at least coincidental with, permitted "secondary" overlay uses. It is not the intent of this district to authorize the establishment of a new area within the City for an independent commercial center or district. Rather, it is the intent of this district to recognize the special need and character of a planned exposition, convention, and conference facility, and to make provision for this special need and character in an appropriate location.

B. Principal Permitted Uses

[existing content all removed]

See the underlying zoning district CW City West for Principal Permitted uses, standards and regulations (Section 3.1.30).

C. Special Land Use (EXO overlay uses)

The following uses are permitted as part of an EXO Overlay subject to Section ~~3.33~~ 3.19, Section ~~3.34~~ 3.20, and Section 3.25: The supplemental conditions of Section 3.25 shall govern in the event of a conflict.

i. Exposition, conference, and convention facilities

The following uses, when part of a development that includes a use described in subsection C above:

~~ii. Hotels and motels~~

~~iii. Museums~~

~~iv. Theaters~~

~~v. Places of worship~~

~~vi. Restaurants (sit-down, but not fast food sit-down)~~

vii. Indoor and outdoor recreational facilities

viii. Retail sale of products or services

ix. Onsite support retail uses

x. Outside exhibits, fairs, entertainment and festivals

xi. Surface parking lots

D. Development Standards

Lot Size

Minimum lot area: See section 3.25

Minimum lot width: Not specified

Lot Coverage

Maximum FAR: 0.5 FAR

Setbacks

Minimum front yard setback: 50 ft or height of building*

Minimum rear yard setback: 50 ft or height of building*

Minimum side yard setback: 50 ft or height of building*

Building Height

Maximum building height: 65 ft or 5 stories, whichever is less

Floor area

Minimum floor area: See Section 3.25

Parking setbacks

Minimum front yard setback: 20 ft

Minimum rear yard setback: 20 ft

Minimum side yard setback: 20 ft

Notes

For additions to the above requirements, refer to Section 3.6.2 Notes to District Standards: E, M, P, and Q

See Selected references below for applicability

*See section 3.25.2.F for further regulations

Selected references

3. Zoning Districts

~~-OST Retail Service Overlay~~

~~-OST District Regulations and Required Conditions~~

- CW City West

- MDO Mixed-Use Development Option

- EXO Overlay District Requirements

Subsections 4-7. [unchanged]

Part XV.

That the City of Novi Zoning Ordinance, as amended, Article 3, Zoning Districts, Section 3.25 EXO Exposition Overlay District required conditions, is hereby amended to read as follows:

Section 3.25 EXO Overlay district required conditions

1. EXO Overlay required conditions

A. [unchanged]

B. An EXO Overlay district shall satisfy all of the following in order to qualify for approval:

i.-iv. [unchanged]

v. The zoning classification of the entire EXO Overlay district shall be ~~OST, Planned Office Service Technology-CW, City West district.~~

vi. [unchanged]

2. Supplemental Required Conditions for Overlay Uses. In the event of construction of building(s) for overlay uses permitted under Section 3.1.15.C, the following supplemental required conditions shall apply:

A.-K. [unchanged]

L. Approval Process

i. Approval of an EXO Overlay district shall require the approval of a Zoning Ordinance amendment to amend the Zoning Map by the City Council, in the exercise of its legislative discretion, following a public hearing held by, and receipt of a recommendation from, the Planning Commission. An applicant seeking approval of an EXO Overlay district rezoning shall submit a rezoning petition to the City. If approved, the amendment shall place, or overlay, the EXO Overlay district over the underlying CW or OST district relative to the property which is the subject of the amendment.

ii.-iii. [unchanged]

PART XVI. Severability. Should any section, subdivision, clause, or phrase of this Ordinance be declared by the courts to be invalid, the validity of the Ordinance as a whole, or in part, shall not be affected other than the part invalidated.

PART XVII. Savings Clause. The amendment of the Novi Code of Ordinances set forth in this Ordinance does not affect or impair any act done, offense committed, or right accruing, accrued, or acquired or liability, penalty, forfeiture or punishment, pending or

incurred prior to the amendment of the Novi Code of Ordinances set forth in this Ordinance.

PART XVIII. Repealer. All other Ordinance or parts of Ordinance in conflict herewith are hereby repealed only to the extent necessary to give this Ordinance full force and effect.

PART XIX. Effective Date: Publication. Public hearing having been held hereon pursuant to the provisions of Section 103 of Act 110 of the Public Acts of 2006, as amended, the provisions of this Ordinance shall be published within fifteen (15) days of its adoption by publication of a brief notice in a newspaper circulated in the City of Novi stating the date of enactment and effective date, a brief statement as to its regulatory effect and that a complete copy of the Ordinance is available for public purchase, use and inspection at the office of the City Clerk during the hours of 8:00 A.M. to 5:00 P.M., Local Time. The provisions of this Ordinance shall become effective seven (7) days after its publication.

MADE, PASSED, AND ADOPTED BY THE CITY COUNCIL OF THE CITY OF NOVI, OAKLAND COUNTY, MICHIGAN, ON THE ____ DAY OF _____, 2023.

ROBERT J. GATT, MAYOR

CORTNEY HANSON, CITY CLERK

Ayes:

Nays:

Abstentions:

Absent:

CITY WEST DESIGN GUIDE – DRAFT

City West Design Guide

A Design Guide to accompany sections 3.33
and 3.34 of the City of Novi Zoning Ordinance

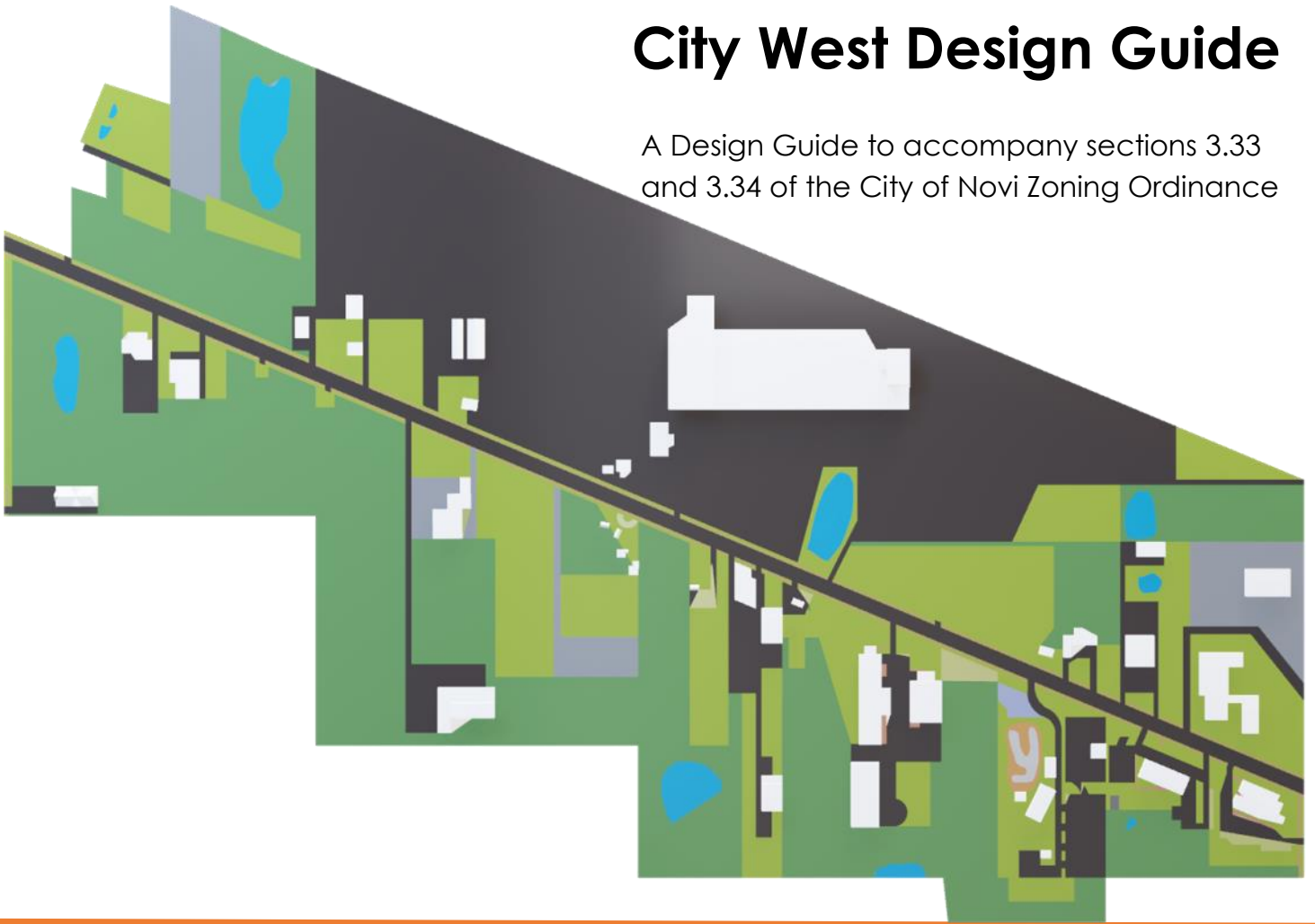


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- A. Executive Summary
- B. Background & Existing Conditions
- C. Vision and Guiding Principles
- D. Form Based Design & Inspiration Images
- E. Recommended Design Guidelines



A. Executive Summary

The City West Design Guide is meant to provide guidance and visual representation of the regulations for the City West District in addition to providing design features and concepts recommended to be included in development projects. The goal of the City West Design Guide is to facilitate development that includes high-quality project design and building materials, and works to create a vibrant, people-oriented district in the City of Novi.

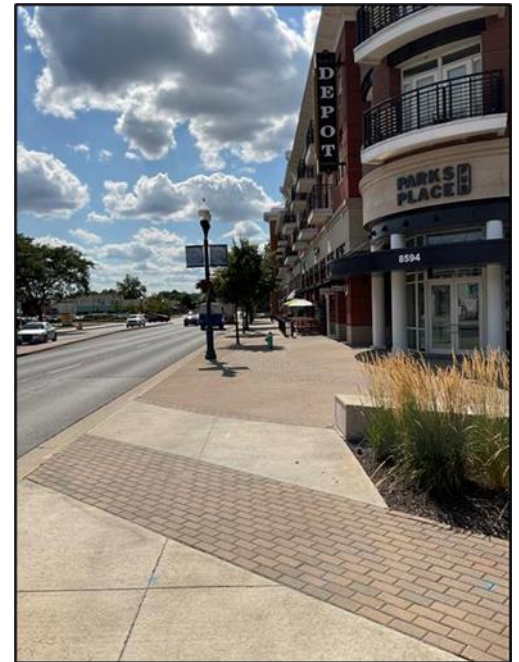


Figure A1: mixed-use development and streetscape in Fishers, Indiana

“This area offers the potential for the creation of a prominent new district combining entertainment, convention, commercial, office and residential uses in a cohesive, high-density, walkable pattern.”

- 2016 Master Plan

Figure A2 (below) - Fishers, Indiana mixed-use development; images of Indiana communities courtesy of Rowan Brady of Beckett and Raeder, Inc.

The guide is composed of images of developments from Novi and other communities, maps representing the current conditions of the City West area, and 3D models to illustrate standards of this district. Images and renderings are meant to serve as examples, while the Zoning Ordinance contains the actual development regulations, standards and procedures. Inspiration for the vision and guiding principles is based on the recommendations in the 2016 Master Plan and Grand River Corridor Study.



B. Background & Existing Conditions

- B.1 — Land Use
- B.2 — Public Facilities
- B.3 — Zoning
- B.4 — Utilities



Figure B1 (below): aerial of both City West areas



Figure B2 (above): aerial of the western portion of the City West North area , outlined in blue



Figure B3 (above): aerial of the City West South area, outlined in green

B. Background & Existing Conditions

B.1 — Land Use: Description of the District

The City West district is situated southeast of the intersection of Interstate 96 and Beck Road, bounded by I-96 to the north, Taft Road on the east, residential communities to the south, and Beck Road on the west. The historic Grand River Avenue corridor, running from downtown Detroit all the way to Lake Michigan on the west side of the state, bisects the City West area. In this area of the city, Grand River is an auto-centric five-lane major arterial with a speed limit of 50 miles per hour. A mix of older office, industrial and manufacturing buildings, and vacant properties are present on either side. These characteristics make the streetscape inhospitable for pedestrians and cyclists.

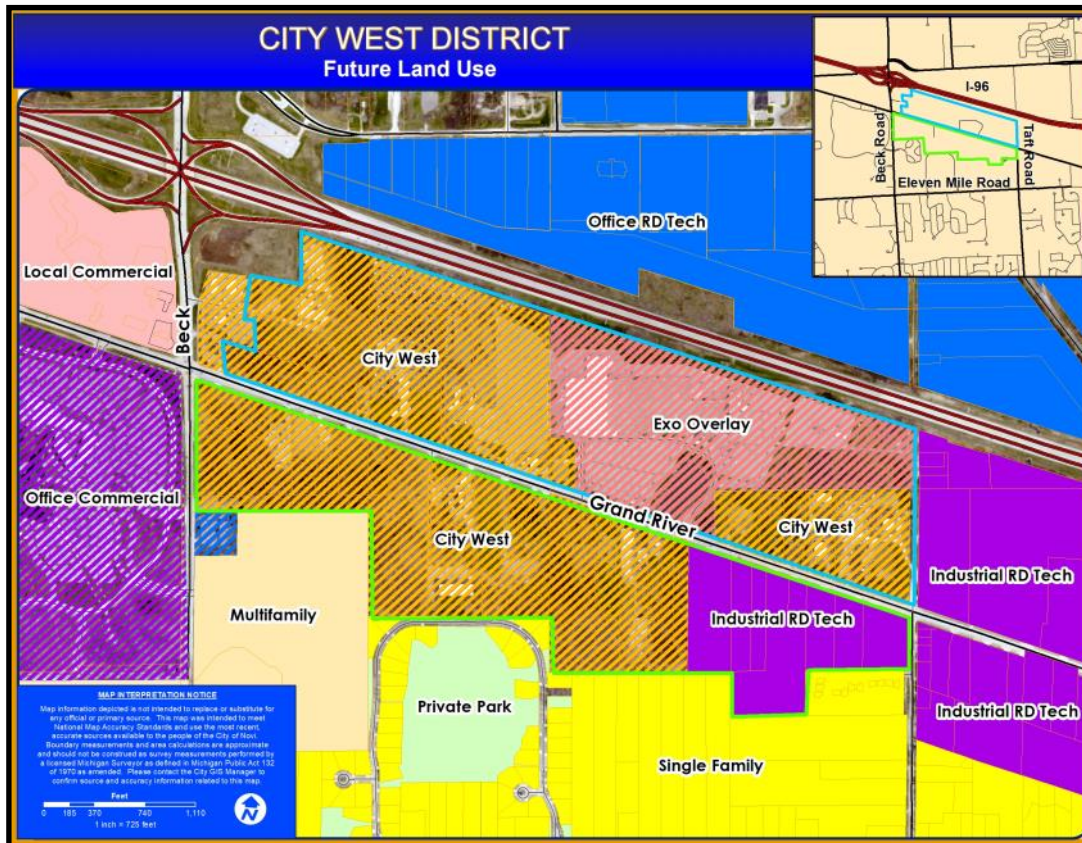


Figure B4 City West Future Land Use Map

Overall, the total land area included in the City West District is approximately 200 acres. Approximately 68.6 acres are vacant or a non-conforming residential use. Parcels developed with industrial uses that are not consistent with the desired future use for City West make up a total of about 55 acres. Destinations nearby in the corridor include Providence Park Hospital System and Suburban Collection Showplace, which are significant economic drivers in Novi, providing jobs and bringing in visitors.



Figure B5 Providence Park Hospital;
Image source: Novi Chamber of Commerce

B. Background & Existing Conditions

B.1 — Land Use: Natural Features

A map of the natural features shows the presence of both woodlands and wetlands in the area. A significant amount of woodlands and wetlands are present in the City West South area, while there are just wetlands located in the City West North area.

The City West text allows for **up to 50 percent** of the required open space to consist of regulated woodlands and/or wetlands if they are permanently protected by conservation easements (3.33.H).

“Wherever natural open spaces shall be created or preserved, they shall be physically connected, when possible, to adjacent natural open space areas.”

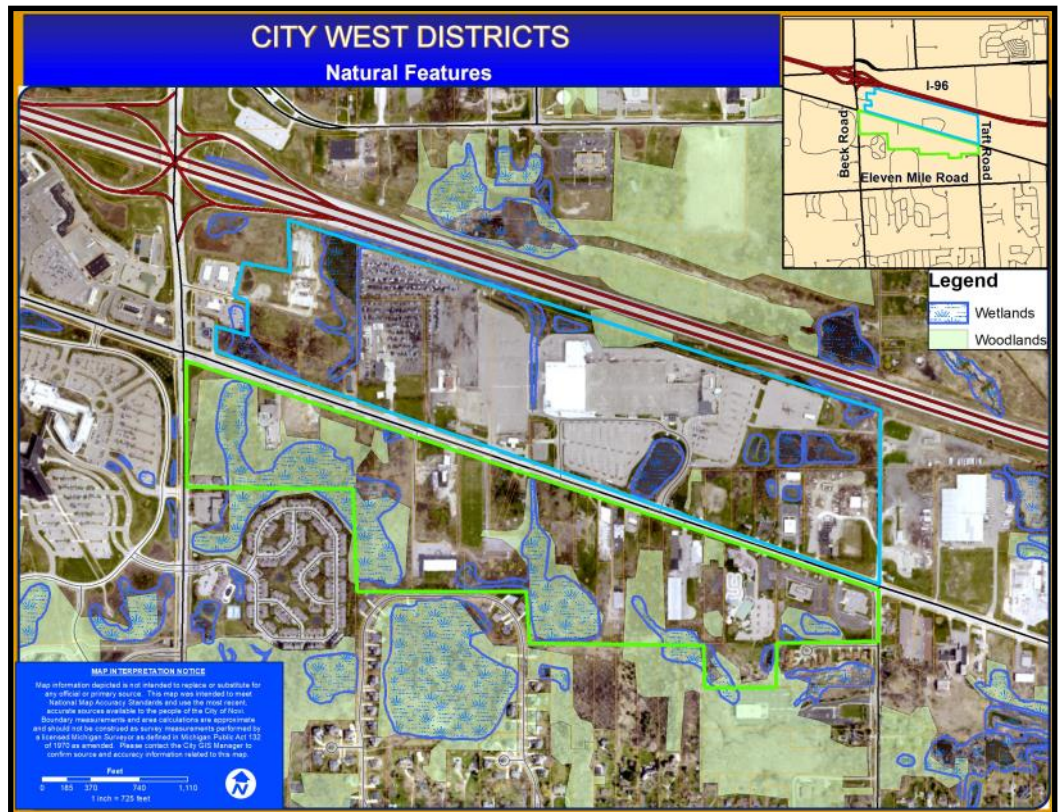


Figure B6 City West Natural Features Map, woodlands are denoted in light green while wetlands are colored in blue.



Figure B7: aerial of City West facing east

The aerial to the left shows City West facing east down Grand River. Two large ponds (circled in yellow) are present on either side of Grand River, and the difference in woodland concentration between north and south is evident. Buffers and setbacks from natural features help preserve, protect, and maintain quality of woodlands, wetlands, and streams within the district.

B. Background & Existing Conditions

B.2 — Public Facilities

Public Facilities shown in Figure B8 to the right include the sidewalk inventory of the district — 8-foot sidewalks line both sides of Grand River from Taft to Beck Road. Other facilities of note include traffic signals and street lights, which both can be found at important intersections. Road crossing difficulty, established in the Non-Motorized Transportation Plan, portrays Grand River as category E, the most difficult road category to cross.

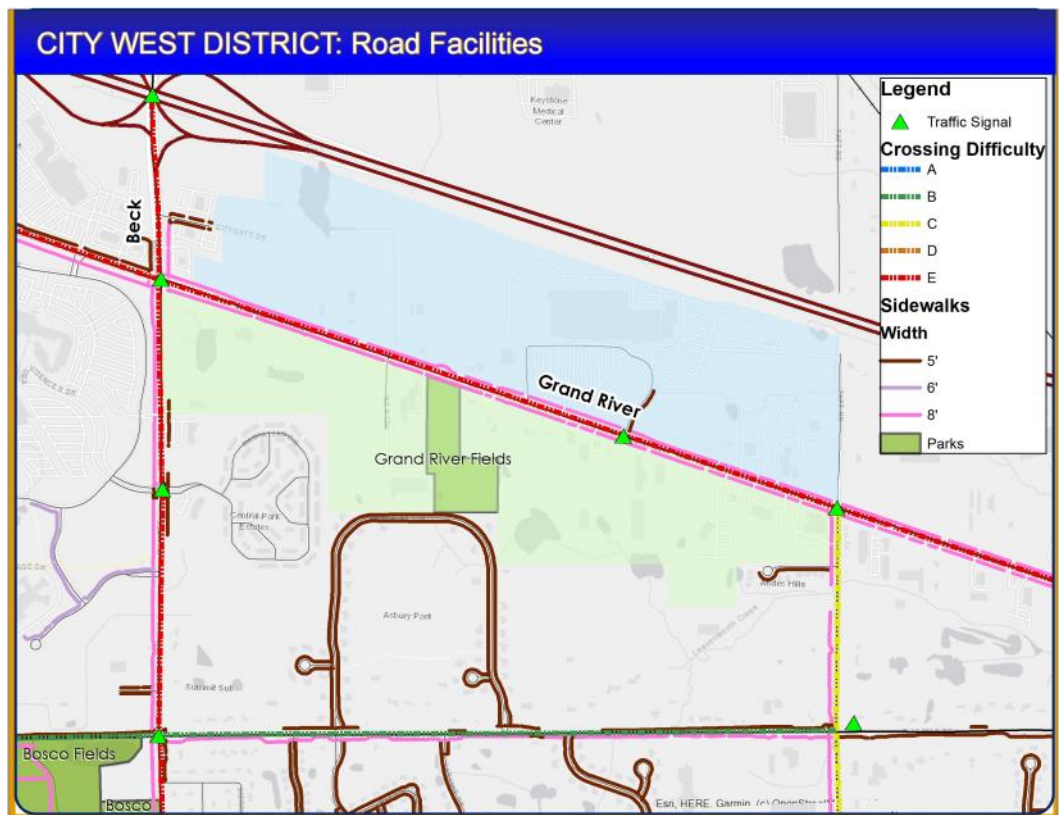


Figure B8: Roads and Sidewalk Facilities map

Beck Road runs north and south, connects with Grand River, and has been identified as a major non-motorized corridor in the City of Novi Non-Motorized Plan. The ITC Trail, depicted below, has been proposed as a regional trail connection crossing Grand River at Beck Road. The Providence Park hospital campus has recently expanded their trail network, and connects with the ITC Trail.



Figure B9: the ITC Trail is a prime example of non-motorized infrastructure in the City of Novi

B. Background & Existing Conditions

The current zoning of the City West area includes six different classifications. Approximately 140 acres are north of Grand River Avenue with 17 parcels included. The majority of the area, 124.96 acres, is currently zoned OST - Office Service Technology. The EXO Exposition Overlay District applies to a 55-acre portion of the Suburban Collection Showplace property. Approximately 10.05 acres are zoned I-1 Light Industrial. To the south of Grand River Avenue, the City West area includes 34 parcels totaling 113.48 acres. The current zoning of the majority of the area is I-1 Light Industrial.

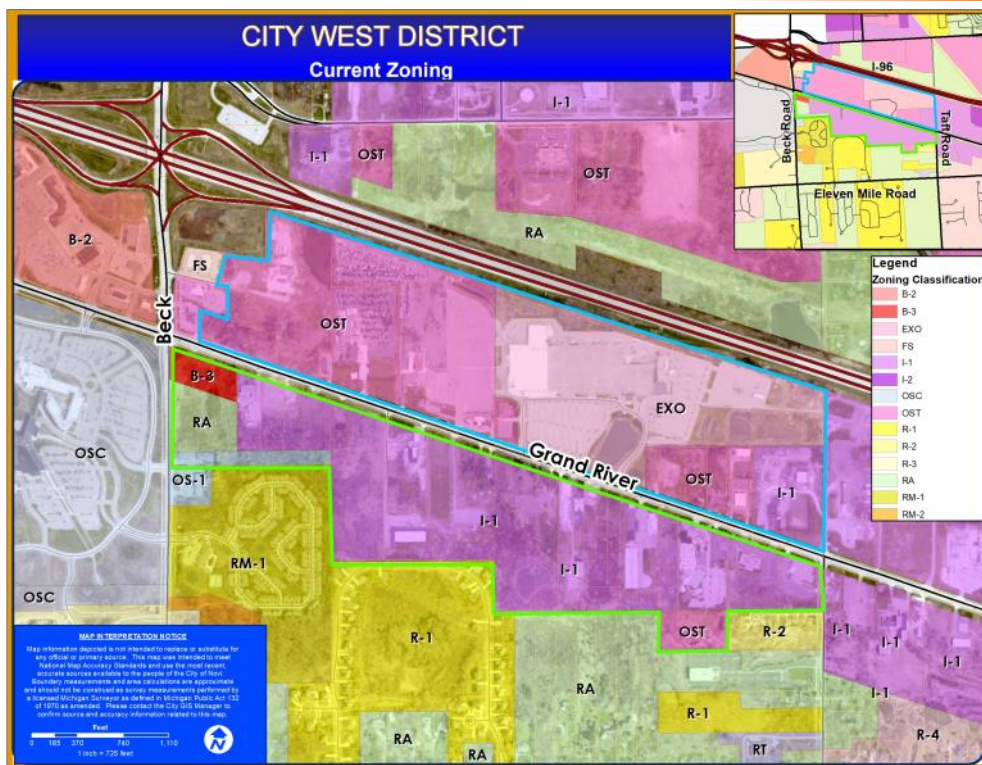


Figure B10: Current Zoning Map of the City West area

One area located at the southeast corner of the intersection of Beck Road and Grand River Avenue is zoned B-3. Fronting on Beck Road, one 6.4 acre area is zoned RA Residential Acreage, and a 3.4 acre parcel is zoned OS-1 Office Service.

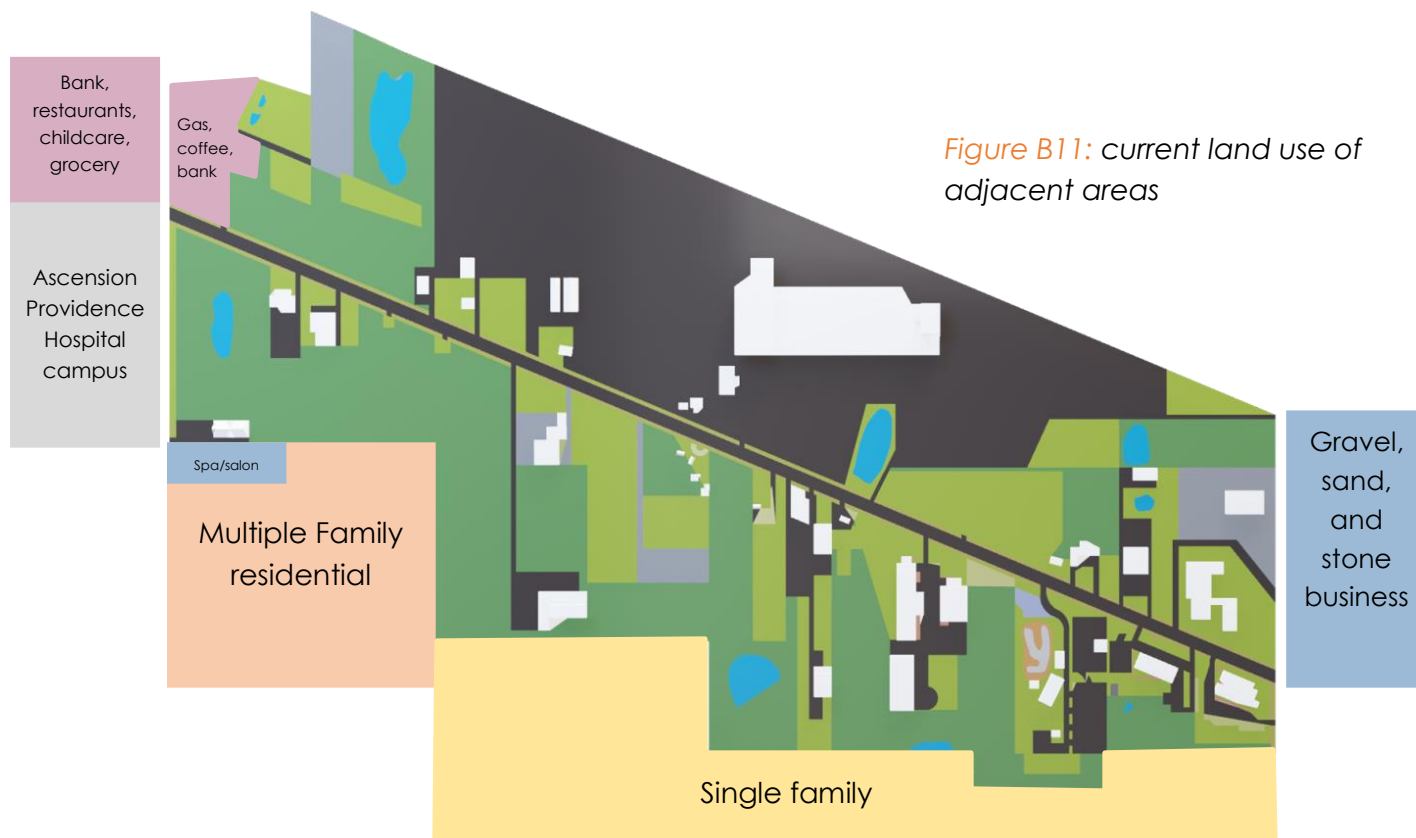


Figure B11: current land use of adjacent areas

B. Background & Existing Conditions

B.4 — Utilities

Development in the City West area will depend on both existing and future utility infrastructure to serve the needs of residents and businesses. Some sanitary sewer improvements will need to be made before all of the area is developed. **Figure B12** below shows the sanitary districts and the locations of the current sanitary gravity mains.

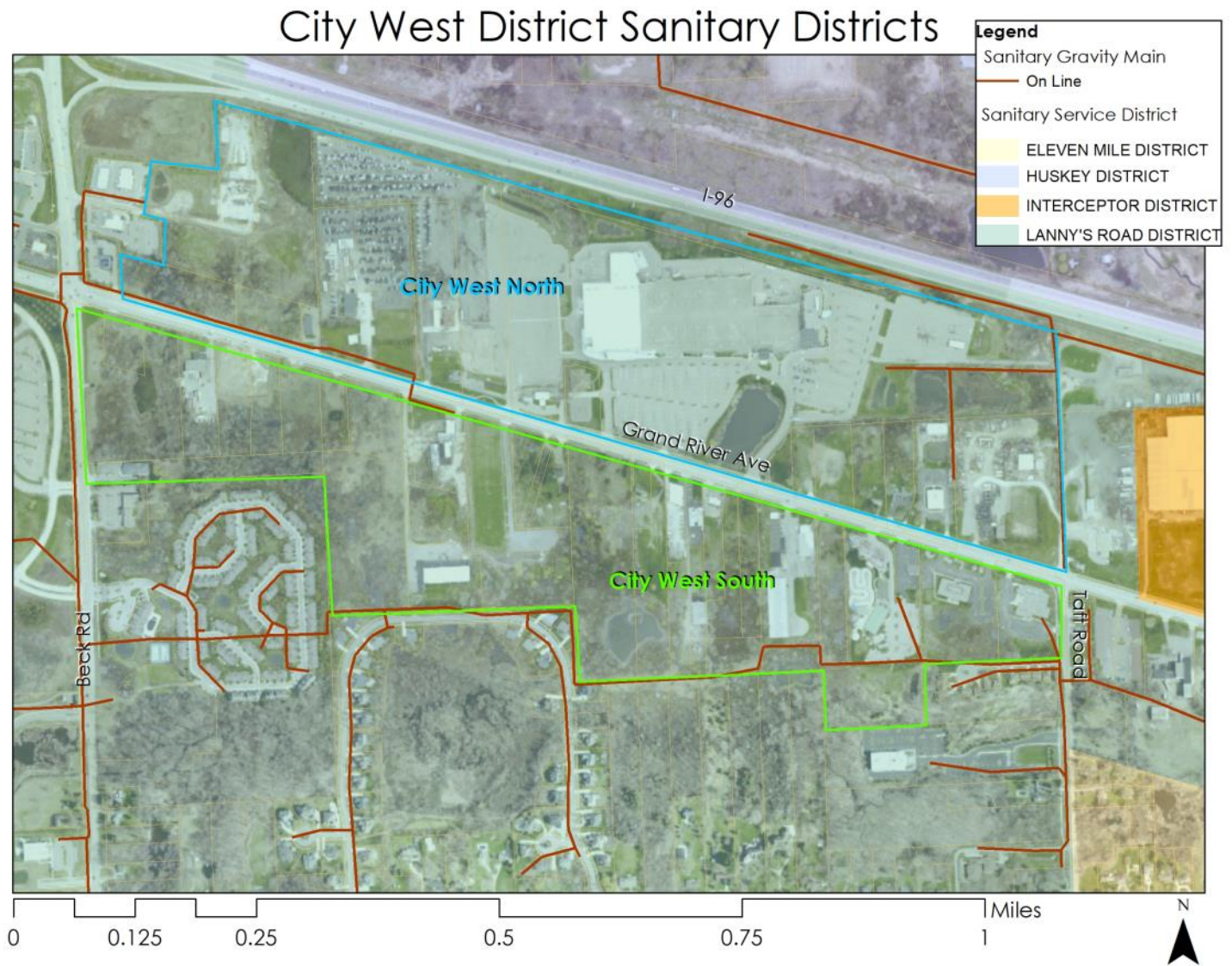


Figure B12: sanitary service districts and sanitary gravity main locations in City West

B. Background & Existing Conditions

B.4 — Utilities

Regional or shared detention basins between developments, in addition to underground detention basins, are encouraged to help consolidate properties and facilitate denser development. **Figure B13** below shows the drainage districts in City West and their respective status. Properties in City West are all located within one of four regional detention districts — the North Novi District and Bosco District, which are proposed but not built, and the Taft Road District and Grand River District, which both have built regional detention.

City West District Stormwater Drainage

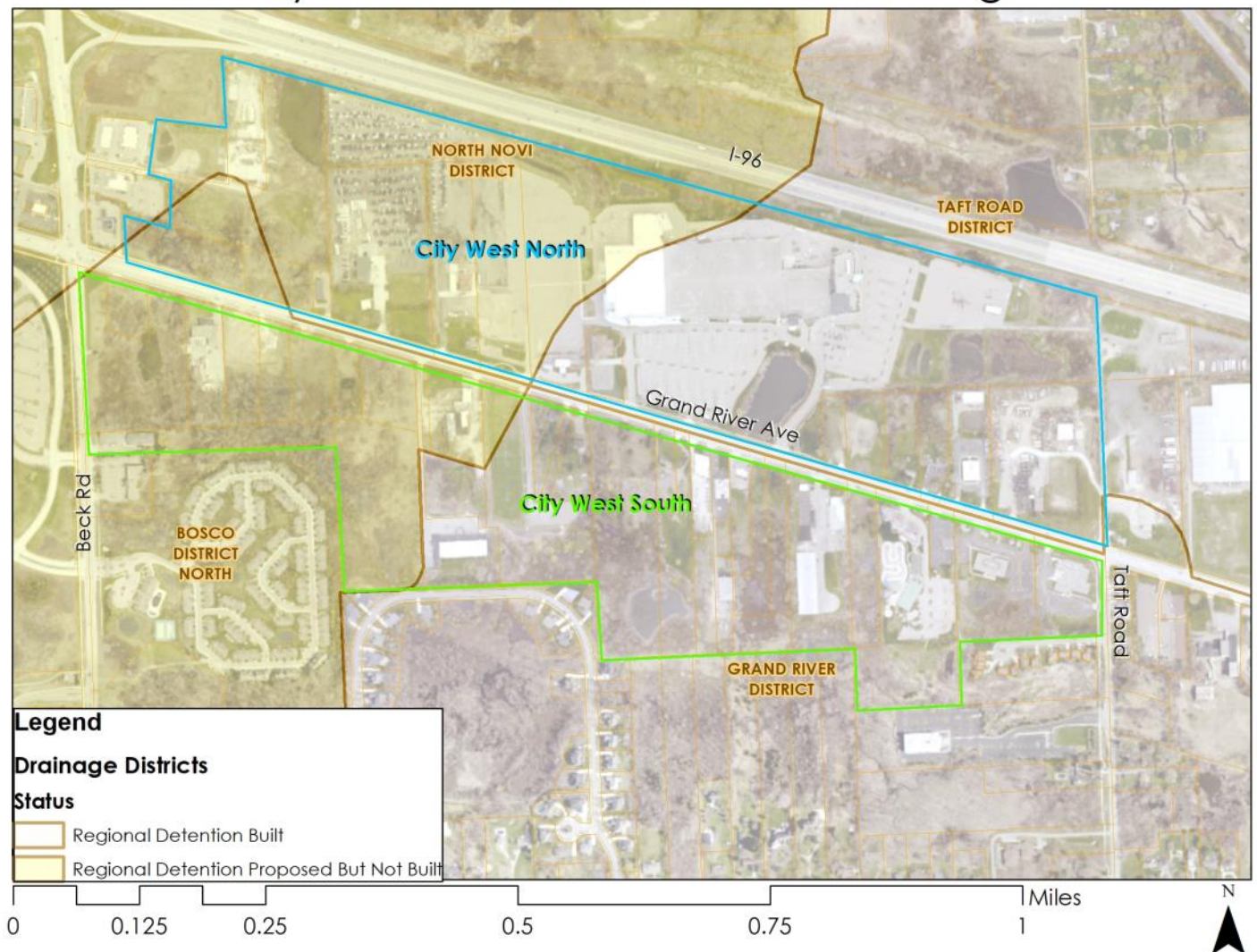


Figure B13: drainage districts and their status in City West

B. Background & Existing Conditions

B.4 — Utilities

Figure B14 below shows current infrastructure for water service in the City West area. All of City West is located within the intermediate water pressure district. There is water provided on both the north and south sides of Grand River.

City West District Water Service



Figure B14: water main locations in City West

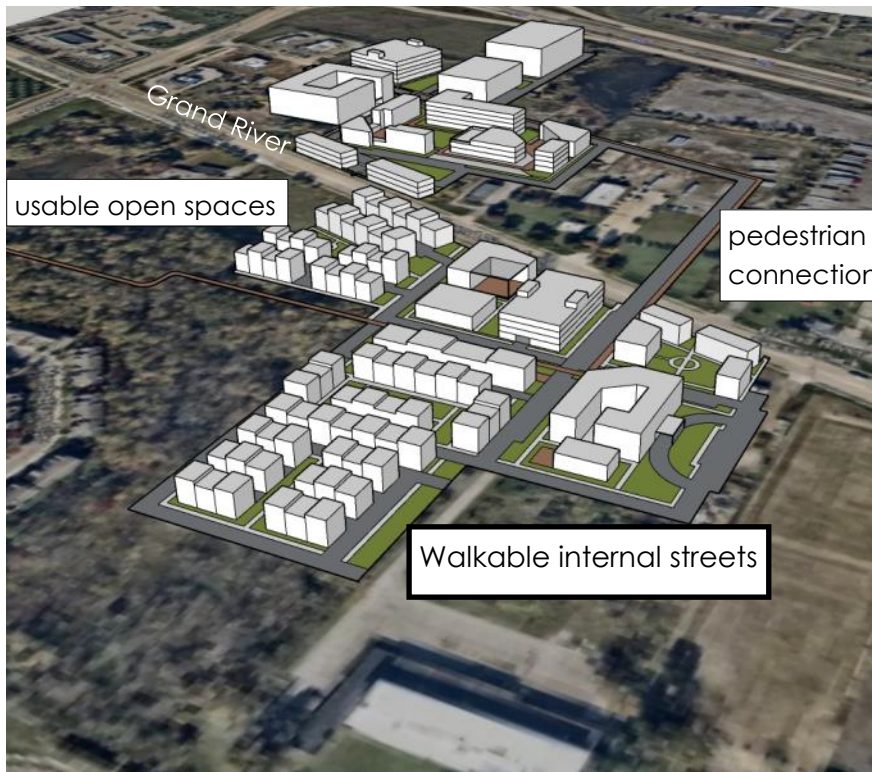
C. Vision and Guiding Principles

C.1 — General Vision

Figures C1 and C2 below illustrate the vision for City West in terms of building form north and south of Grand River — taller buildings in the City West North area, especially closer to I-96, and scaled down heights in the City West South area. Shared access and parking, public gathering spaces, pedestrian and vehicle connectivity, and preservation of important natural features are also depicted.

Parking lots are located in the side and back of buildings with the intent of framing both Grand River and the collector streets with buildings.

Models within the Design Guide are purely conceptual and do not assume any changes to existing standards



Figures C1 (above) and C2 (left): aerial views of a part of City West with 3D model example developments overlaid. Additional development anticipated on other parcels.

It is strongly encouraged that internal access drive networks shall be established and utilized as local streets which are to be privately owned and maintained.

Buildings are to be oriented towards the primary pedestrian street

C. Vision and Guiding Principles

C.1 — General Vision

Excerpt from the 2016 Master Plan Update describing City West:

The district is envisioned as a distinct neighborhood as well as a complement to major nearby uses such as the Suburban Collection Showplace and the hospital....Creating a vibrant restaurant and entertainment scene that spills into outdoor patios and open spaces is a major goal of this redevelopment strategy.



Figure C4 (above): rendering (by Gensler) of an outdoor gathering space at San Pedro Plaza, Los Angeles.



City West District developments are expected to be designed with pedestrians in mind. Walkable connections between businesses, residences, and entertainment options should provide people with the ability to interact with one another in public areas such as the photos shown to the left and below. The images included on this page provide examples of the kind of public gathering spaces and density that is meant to take place in the City West District.

Figure C3 (below): outdoor dining in front of a vertical mixed-use development. Photo courtesy of Bethesda Magazine



Figure C5 (left): lighting and pedestrian activity at night at The BLVD in Lancaster County, California. Photo courtesy of the Congress for New Urbanism

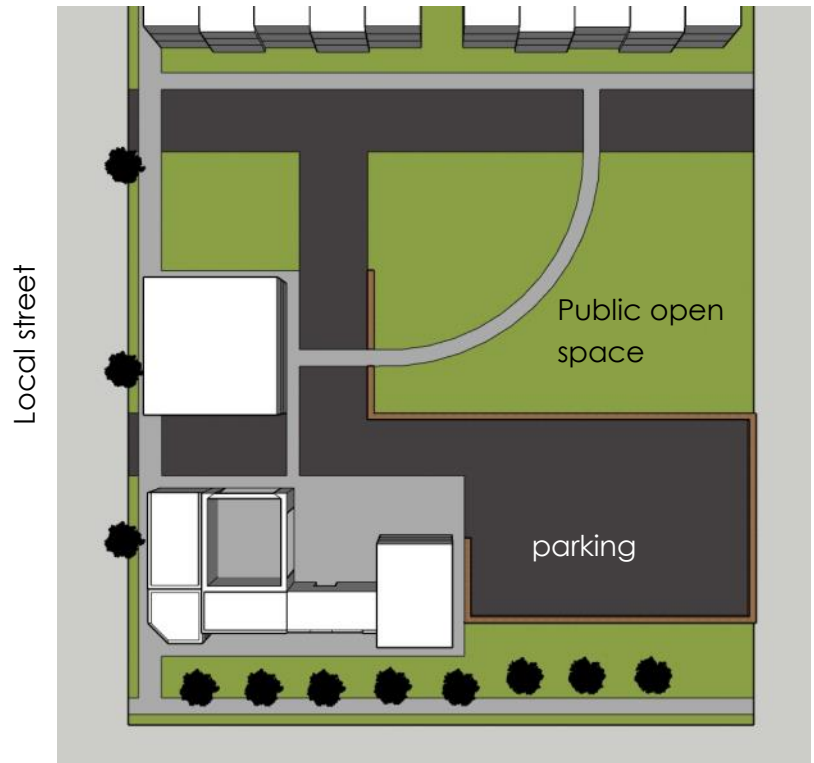
C. Vision and Guiding Principles

C.2 — Horizontal Mixed Use

Horizontal mixed-use refers to developments where the site contains separate residential and non-residential buildings, as opposed to vertical mixed-use, where multiple uses are integrated into a single building. Both types of mixed-use developments are encouraged and permitted in the City West District.

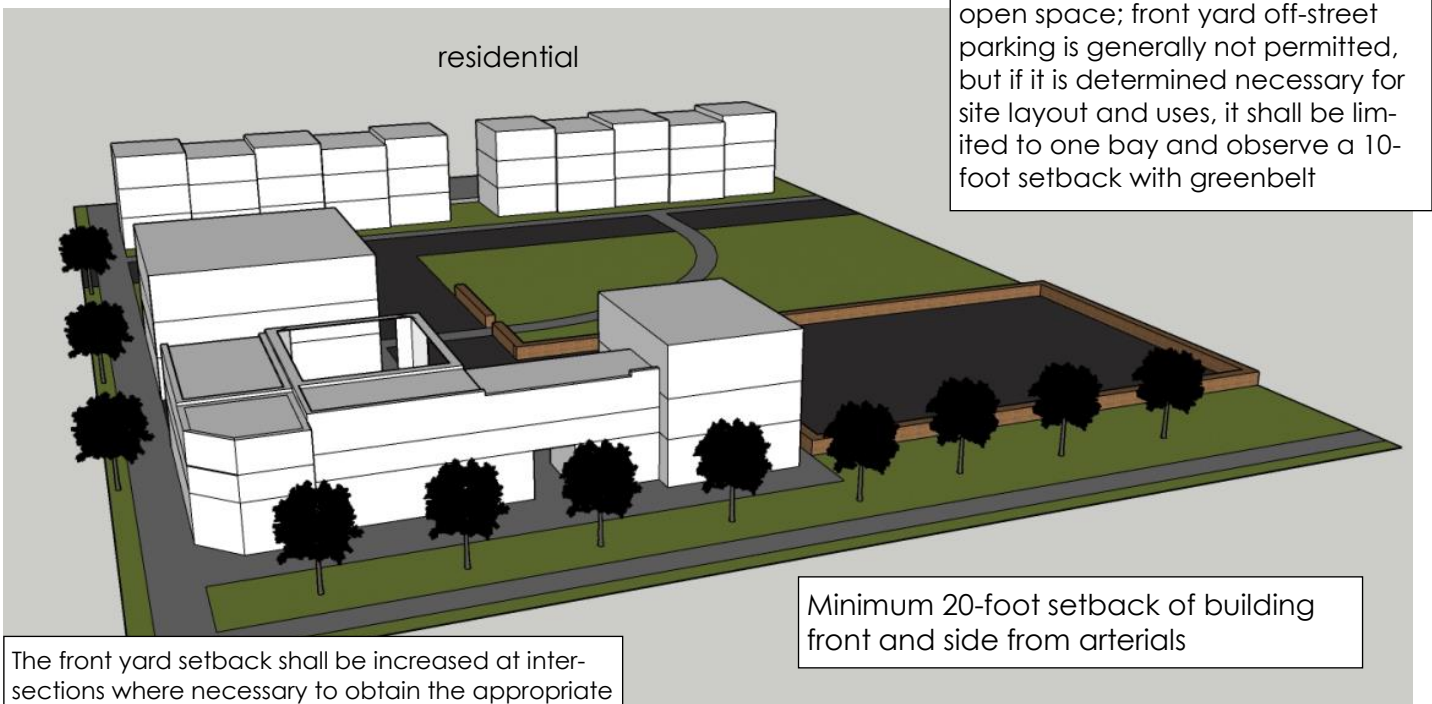
For the Mixed-Use Development Option (MDO), the development must have at least 300 feet in public road frontage

Maximum building setbacks of 20 feet from nonresidential collectors and local streets are meant to create a building presence along the street connected by sidewalks. **Setbacks are measured from the future Right of Way.**



Major arterial (Grand River, Beck, or Taft)

Figures C6 (top) and C7 (bottom): plan views of example horizontal mixed-use model



At least 15% of the development is open space; front yard off-street parking is generally not permitted, but if it is determined necessary for site layout and uses, it shall be limited to one bay and observe a 10-foot setback with greenbelt

Minimum 20-foot setback of building front and side from arterials

The front yard setback shall be increased at intersections where necessary to obtain the appropriate clear vision area for vehicular traffic.

C. Vision and Guiding Principles

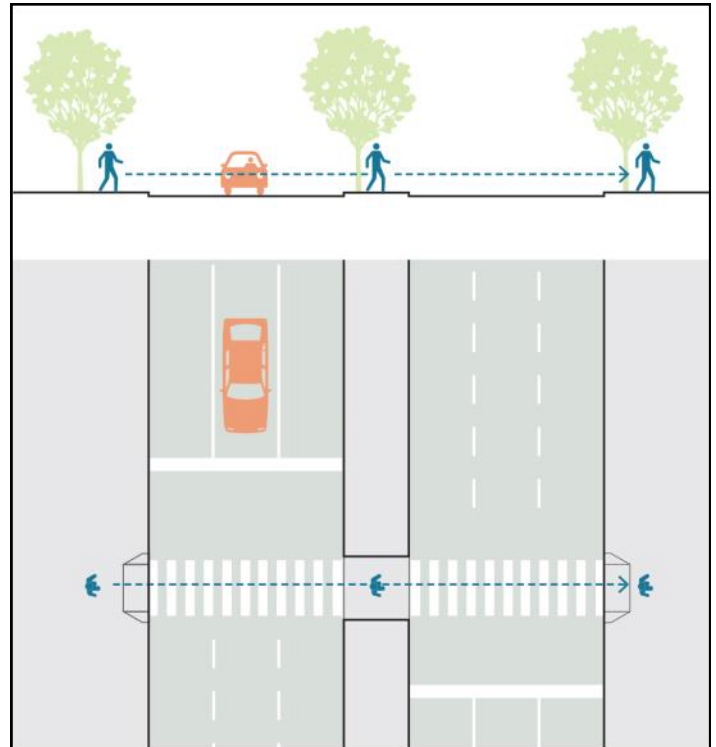
C.3 — Pedestrian Focus - Street Design

The City of Novi is committed to take part in capital projects that result in traffic-calming and a more pedestrian-oriented streetscape. Nonmotorized transportation infrastructure remains an important part in the City's goals. As it currently stands, Grand River Avenue consists of high volumes of fast-moving traffic that is not conducive to people on foot or on bike.

The diagram on the right portrays a midblock crossing and pedestrian refuge that allows for the crossing of a road like Grand River. These kinds of measures increase the ease and safety of crossing busy thoroughfares.

Sidewalks with a minimum of 6 feet are required abutting any street or internal road. Along major thoroughfares sidewalks shall be 8 to 10 feet.

Figure C8: diagram of a midblock crossing; image courtesy of the Global Designing Cities Initiative



It is intended the internal street network and local roads created would be designed to focus the activity **off** Grand River. However, it will be necessary to provide additional connections between the north and south sides of the district. The conceptual design of Grand River shown below in Figure C9 shows available options for pedestrian-oriented changes on Grand River.



Figure C9: new design of a section of Grand River Avenue in the City of Detroit

C. Vision and Guiding Principles

C.3 — Pedestrian Focus - Street Design

Changes to and along Grand River are expected to align with the development of this denser, pedestrian-focused corridor. To connect the north and south sides of Grand River, there will be options of implementing an at-grade crosswalk with a traffic light or potentially the construction of a pedestrian bridge as seen in Figure C10 below.

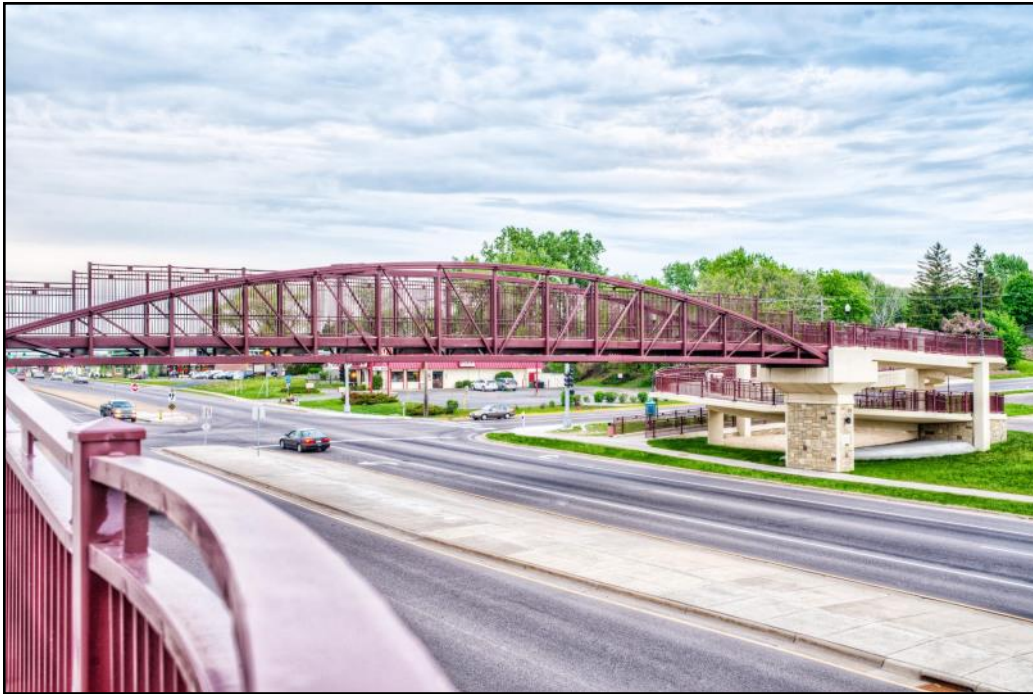


Figure C10: Pedestrian bridge with accessible switchback ramps in Columbia Heights, Minnesota. Photo courtesy of C.S. McCrossan

Another expected change to Grand River Avenue is the addition of bus traffic, with stops at Providence Park and potentially somewhere in City West. A transit stop along Grand River would likely include covered seating and a design that complements the district and the City of Novi.



Figure C11: bus shelter in Houston, courtesy of ricedesignalliance.org

D. Form Based Design/Inspiration Images

D.1 — Townhouses, apartment dwelling development and multifamily housing

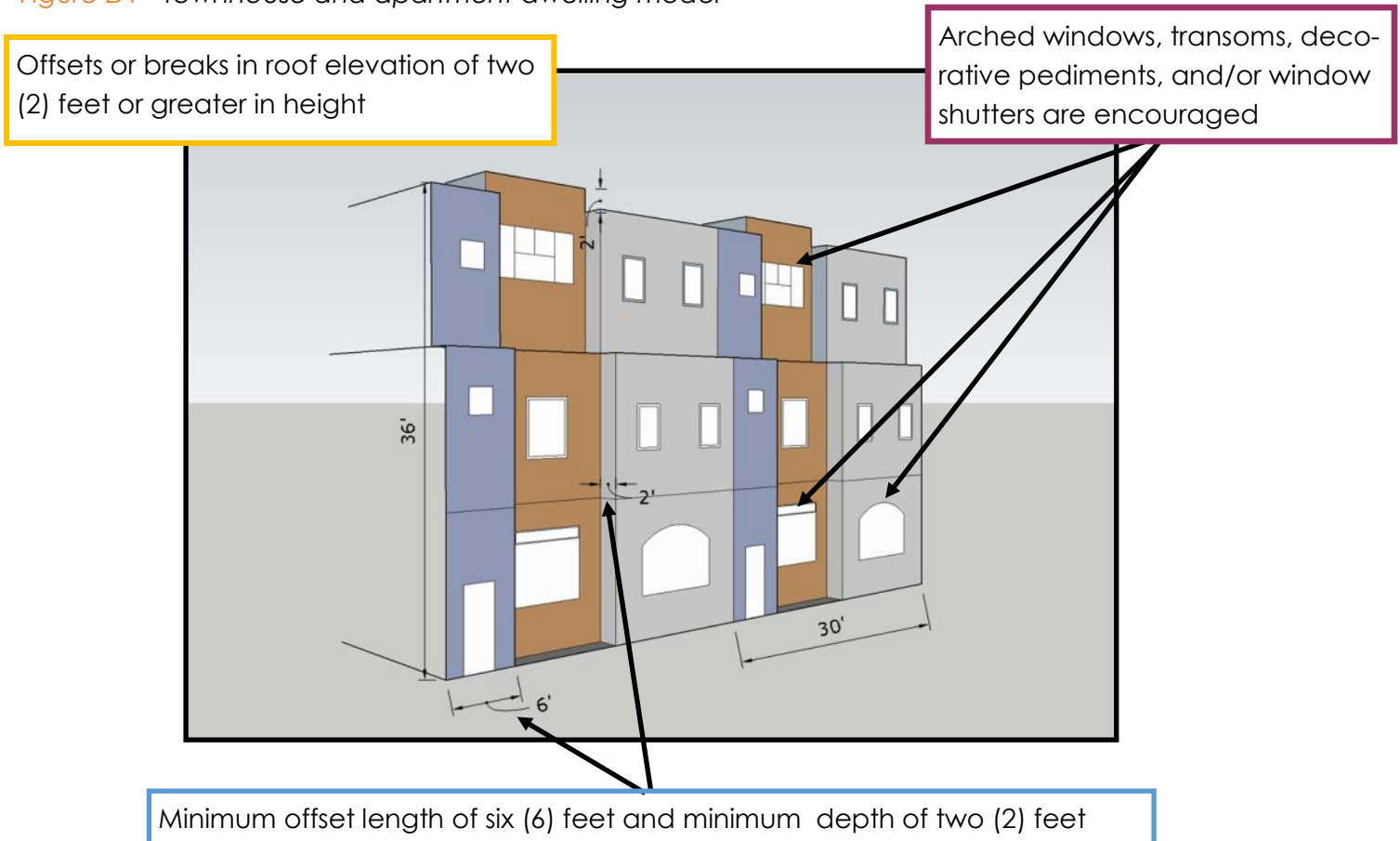
The following text and accompanying images provide guidelines to the development of townhouses, apartment dwellings, and multifamily housing in the MDO.

Wall offsets: A wall offset is defined as a projection or recess of a façade wall of at least two (2) feet in depth. Wall offsets should be incorporated onto those building facades having a length of 40 feet or greater. Wall offsets should be a minimum of six (6) feet in length

Varying roof lines and forms: Offsets or breaks in roof elevations of two (2) feet or greater in height. Incorporate roof pitch and materials of adjacent buildings into carport or garage roofs.

Windows: Decorative window features, such as arched windows, transoms, decorative pediments, and/or window shutters that are sized appropriately and proportionally to the window are encouraged on those facades fronting on streets. The use of recessed windows, moldings, decorative trim and wood frames to add three-dimensional quality and shadow lines to the façade.

Figure D1 - townhouse and apartment dwelling model



D. Form Based Design/Inspiration Images

D.1 — Townhouses, apartment dwelling development and multifamily housing



Images on this page are apartments in Exton, PA, a suburb of Philadelphia. Nearby uses include retail and commercial.

Standalone residential uses can be built under the MDO so long as nonresidential uses are nearby (within 300 feet), and connections to those uses are made.

Figure D2: Apartments in Exton, PA; courtesy of Ashbridge Apartments Facebook page



Figure D3: Amenities at Exton, PA apartments; courtesy of Ashbridge Apartments Facebook page

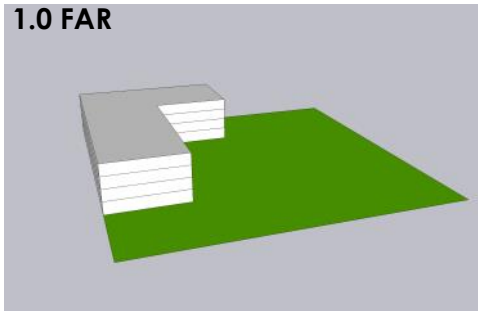
D. Form Based Design/Inspiration Images

D.2 — Building density and intensity

One of the key incentives for utilizing the MDO in the City West District is to permit greater density/intensity. Consult table 3.34.5.D in the City West District text for greater detail. Without the MDO, the maximum Floor Area Ratio in City West is **0.275**. Under the MDO, the density of the net site area for single-use buildings is a maximum FAR of 2.0 in the City West North area and 1.0 in the City West South area. For mixed-use buildings, the maximum FAR is 2.5 for the north area and 1.5 for the south.

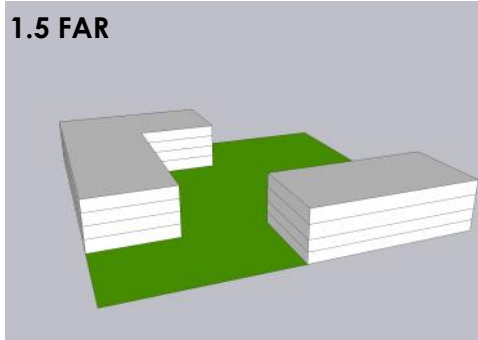
Figure D2: FAR Models depicting potential density available

Single-use buildings in City West South Area →



Four-story building covering a quarter of an acre

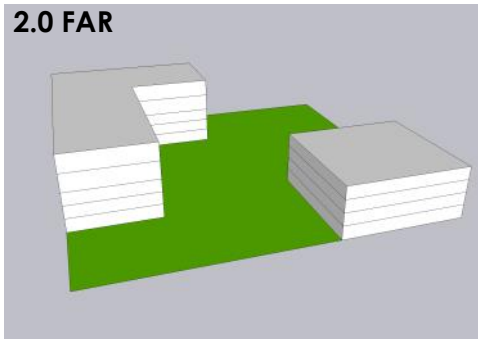
Mixed-use buildings in City West South Area →



Two four-story buildings, one covering a quarter of an acre and the other covering an eighth of an acre

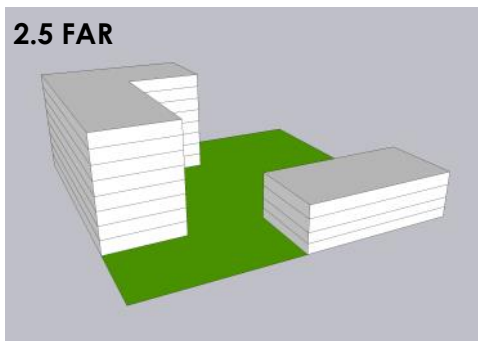
Note that other conditions such as height, parking and open space preservation must be met and may not be accurately portrayed by these models

Single-use buildings in City West North Area →



One six-story building covering a quarter of an acre and one four-story building covering an eighth of an acre

Mixed-use buildings in City West North Area →



One eight-story building covering a quarter of an acre and a four-story building covering an eighth of an acre

D. Form Based Design/Inspiration Images

D.2 — Building density and intensity

The following table is from the City West Mixed Development Option text and refers to the density of development in the district in terms of dwelling units per acre.

3.34.5.D Development Density/Intensity – Based on Net Site Area			
	Residential Only	Mixed-Use Developments	Mixed-Use Developments
Maximum Density ¹		Single Use Building	Multi-Use Building
City West North area	20 du/ac	25 du/ac	30 du/ac
City West South area	10 du/ac	15 du/ac	20 du/ac

¹For all development, density/intensity shall be calculated for the net site area of the development



Haggerty Center/The Liv in Livonia is an example of horizontal mixed-use that includes retail/restaurant and single-use residential buildings on the site. The site has a density of 27.4 du/ac

Figure D3: aerial of Haggerty Center/The Liv



Main Street Village in Novi is an example of a site with a density of 15.26 du/ac. Developments with this density—if incorporated with a nonresidential use on the site—would be permitted in the City West South District

Figure D4: Main Street Village in Novi

D. Form Based Design/Inspiration Images

D.2 — Building density and intensity

The following images show other real-world examples portraying what certain density can look like. Refer to table 3.34.5.D to see the required conditions for differing levels of density.

Figure D5 (right): rendering of The Griffin Novi, by Singh Development

The two renderings shown here are approved developments in Novi—the Griffin Novi (top) and the Bond (bottom). The Griffin will have a density of 23.6 du/ac while the Bond will have a density of 32.5 du/ac.

A residential-only site in the City West North area or a mixed-use site with multi-use buildings in the City West South area permits a maximum 20 du/ac, slightly less than that of The Griffin.

A mixed-use site with multi-use buildings in City West North permits a maximum of 30 du/ac, which would allow for a project such as The Bond.

Figure D7 (right): aerial of Huntley Manor Apartments in Novi. Huntley Manor has an overall density of 7.6 du/ac, which would be permitted in the City West South area in a residential-only site.

NOTE: the minimum acreage for an MDO project is 5 acres, unless otherwise approved by City Council



Figure D6 (above): rendering of The Bond, Humphreys & Partners Architects

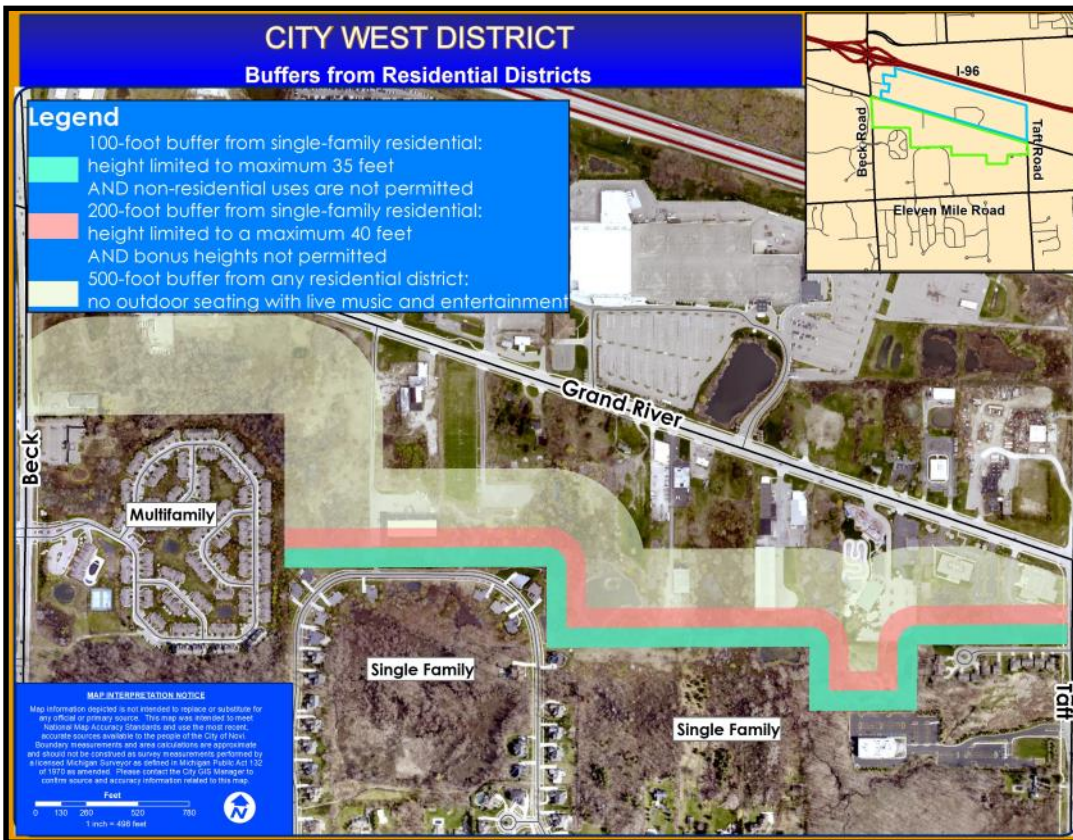


D. Form Based Design/Inspiration Images

D.2 — Density Transition from Residential Districts

The City West District, while intended to be one of the most high-density districts in the City, shall provide a more moderate transition where it abuts Single-Family Residential Districts in City West South as portrayed in Figure B12 below.

Figure D8: map of buffers from residential districts and associated conditions in City West



Where multiple-family abuts a single-family district, minimum building setback shall be two feet for each foot of building height (3.33.1.E.3). Figure D9 below shows apartments 27 feet tall, which would have to be built at least 54 feet away from any single-family district

Figure D9: Main Street Village, pictured to the right, is an example of low-rise multi-family that could be built within a 100-foot buffer of single-family residential uses.



D. Form Based Design/Inspiration Images

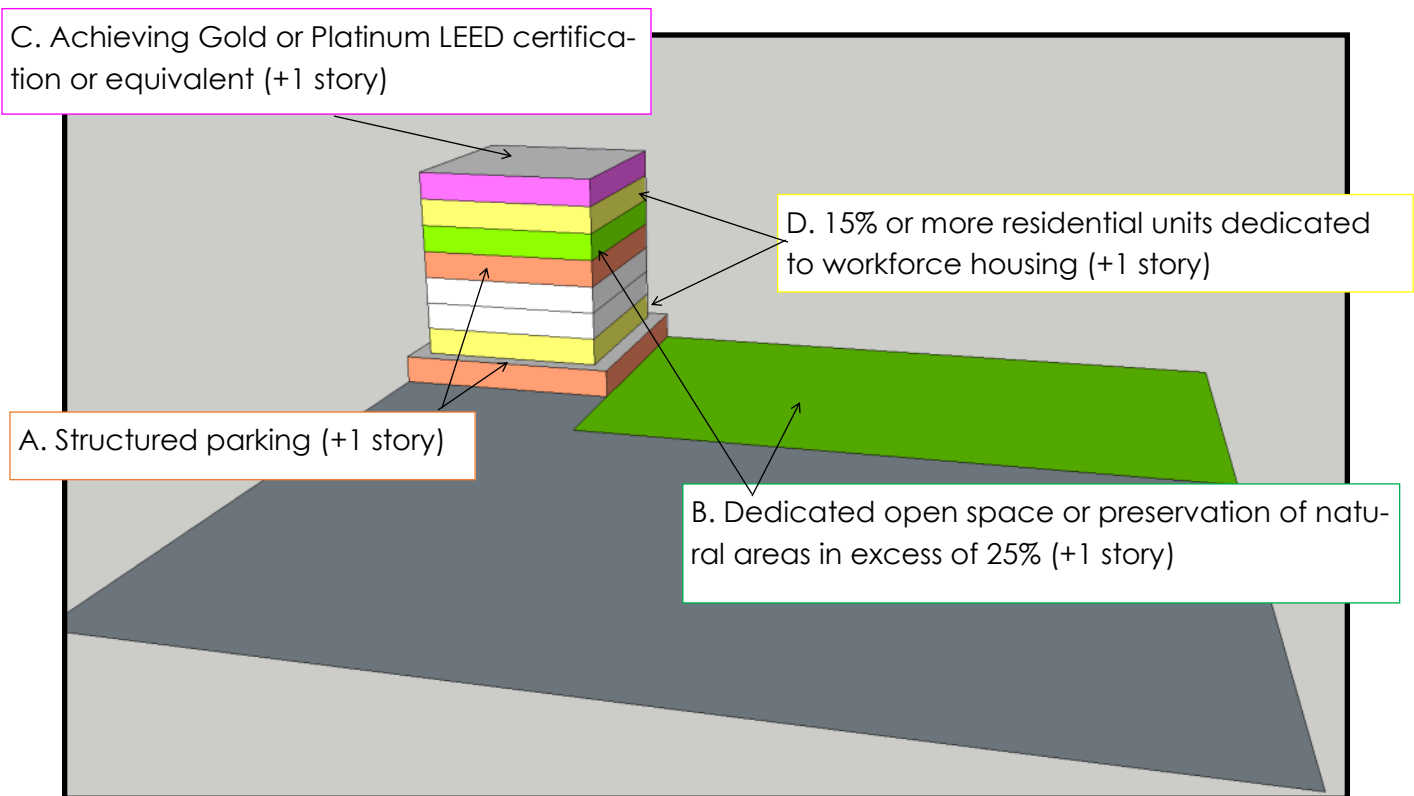
D.3 — Bonus Height

Within the City West District the minimum building height is 2 stories and the maximum building height is 3 stories. With the Mixed Use Development Option, the maximum height for the City West North area is 5 stories and the maximum height for the City West South area is 4 stories, but that **may be exceeded if bonus height conditions are met**.

Maximum building height in the north area may not exceed one-hundred ten (110) feet or 8 stories, whichever is less, and in the south area no building shall exceed 70 feet or 5 stories, whichever is less.

The following model in [Figure D10](#) shows examples of how to achieve greater building height through qualifying for the bonus height conditions as described in 3.34.5.B.

Figure D10: bonus height model



- A. Underground **or** structured parking (up to 2 additional floors—1 story per 125 spaces)
- B. Providing 25% of the total site area as open space; the image above shows the open space consolidated in one location but this does not have to be the case
- C. Applicants shall provide a narrative explaining how sustainability elements have been incorporated and quantify the impacts of those strategies
- D. As described in section 3.34.5.B; the illustration above shows a floor dedicated to workforce housing in yellow and then an additional floor in yellow depicting the bonus story granted for satisfying this condition

D. Form Based Design/Inspiration Images

D.3 — Bonus Height

Figure D11 below shows minimum and maximum heights allowed in City West.

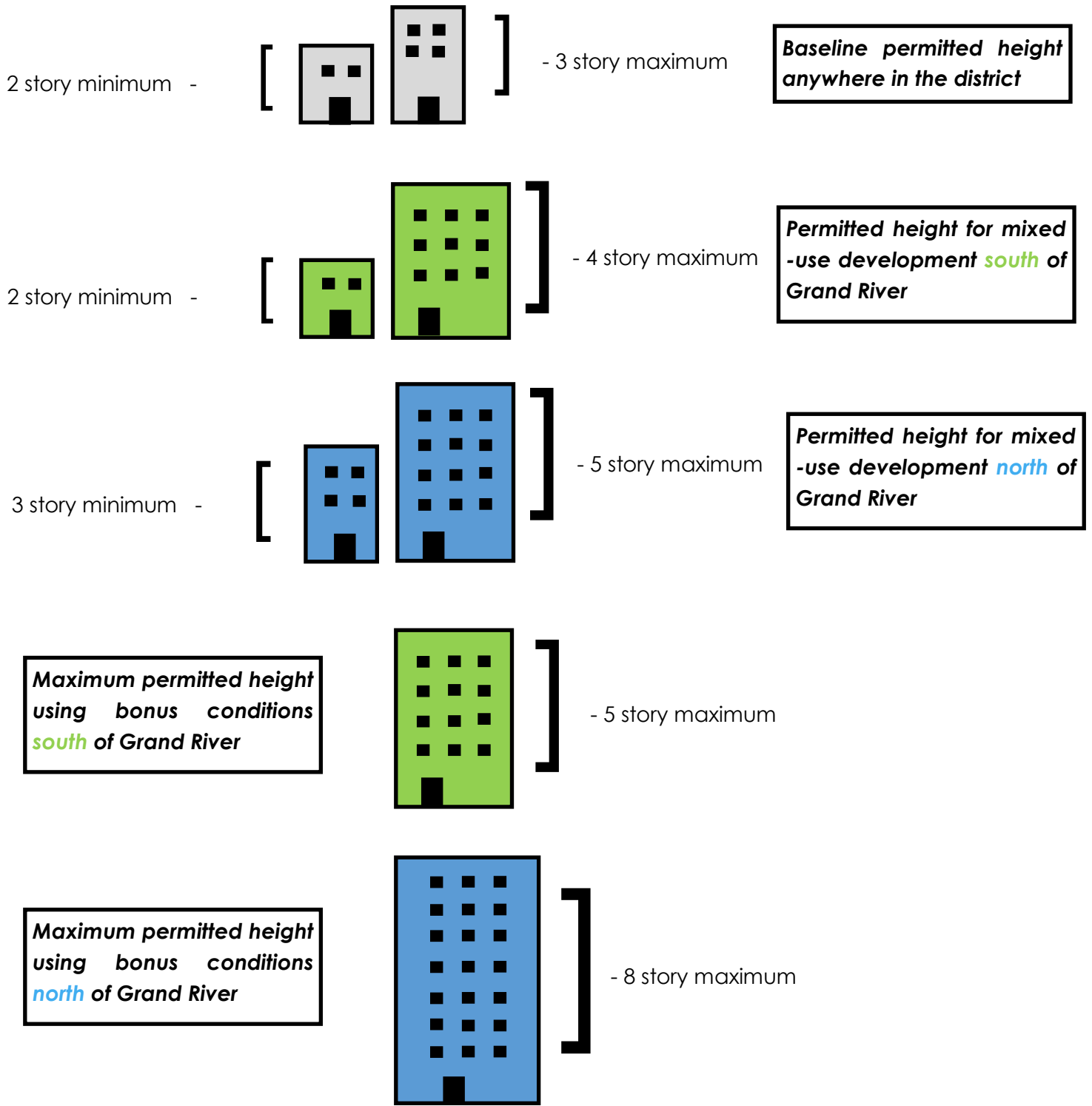
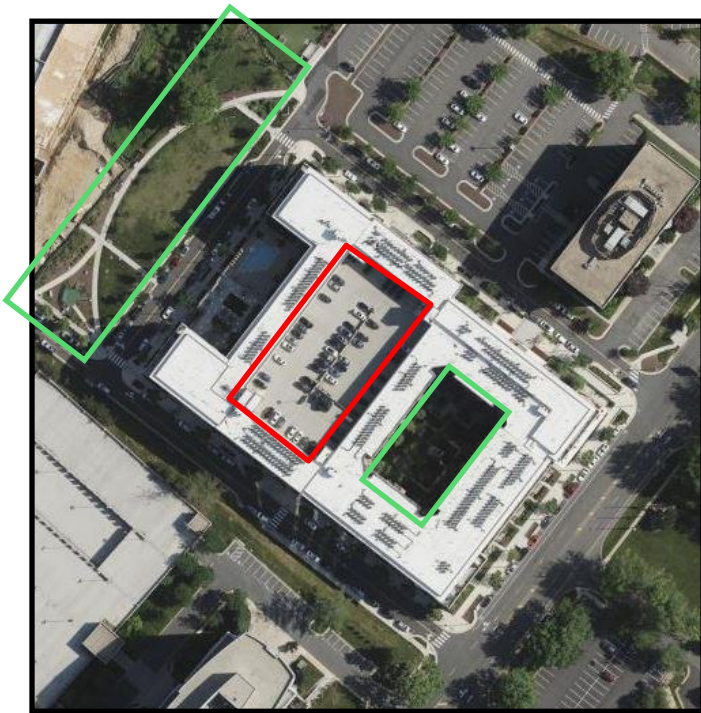


Figure D11: guide to allowable heights in City West

D. Form Based Design/Inspiration Images

D.3 — Bonus Height

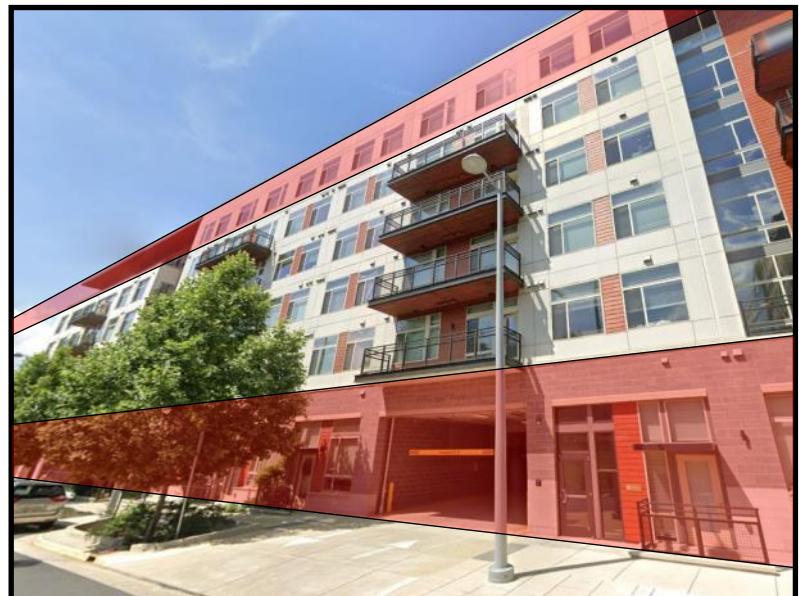
Below are images of Highgate apartments, a residential building in Tysons, Virginia, a suburb of Washington, D.C. Structured parking is provided for residents with the entrance located on the ground floor. This is an example of a way for additional height to be granted through the bonus conditions as described in the City West Ordinance. **Figure D12** shows an aerial of the apartment complex and the parking structure surrounded by the building. **Figure D13** shows the street entrance to the garage and highlights the additional story granted from the inclusion of structured parking. Another way to earn bonus height is open space or preservation of natural areas in excess of 25%.



Allowing taller buildings can reduce the footprint and permit greater preservation of natural features

Figure D12: aerial of Highgate Apartments in Tysons, VA with parking and open space highlighted.

Figure D13: street view of Highgate Apartments with structured parking entrance highlighted. The structured parking on the ground floor and the additional height granted via bonus height are highlighted in the image to the right



D. Form Based Design/Inspiration Images

D.3 — Bonus Height

Achieving gold or platinum LEED Certification or the equivalent can be done in many ways. The provision and use of energy and water efficient design, water conservation, reuse and preservation of resources, and sustainable lifestyle solutions are encouraged to be put into practice through this bonus height consideration. Visit the LEED Certification Levels web page to learn more.



Figure D14: green infrastructure included in the streetscape; source: deeproot.com

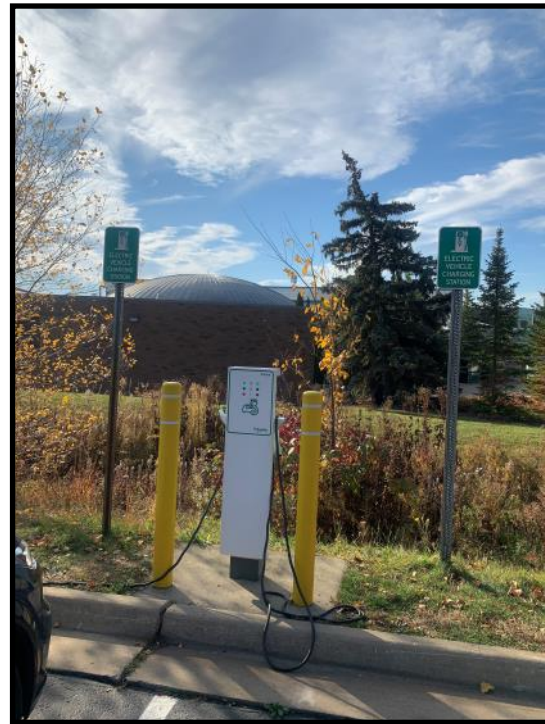


Figure D15: electric vehicle charging stations at the Novi Public Library; see subsection 5.2.15 for details on EV charging stations



Figure D16: solar arrays built in parking lots on Michigan State University's campus

The images on this page are examples of sustainable components that can be constructed on a site. These components are integrated into a site in parking lots or landscaping areas.

D. Form Based Design/Inspiration Images

D.4 — Inspiration Images

The following images are from peer communities or within the City of Novi itself. They contain certain details that could be included in City West developments.



Figure D17 (above) Orenco Station neighborhood in Hillsboro, Oregon



D18 (left): multi-family development in Carmel, Indiana

The landscaping and seating in the image to the right is an example of a public space being made available for nearby residents and visitors to meet. Civita Park also provides fountains and play structures for children, sport courts, picnic areas and native plantings.



Figure D19: public seating and shade structure in San Diego, CA

D. Form Based Design/Inspiration Images

D.4 — Inspiration Images

Figures D18 and D19 show images of a development in Wayzata, Minnesota, composed of senior residential and associated services in the northern half with retail and service businesses located on the ground floors throughout the development.



Figure D20: aerial of The Promenade in Wayzata, Minnesota



Figure D21: public seating and amenity a part of Folkestone, a senior residential community in Wayzata; source: preshomes.org



Figure D22: 618 South Main Apartments in Ann Arbor provide structured parking within the building, high quality building materials, offsets in the architecture, and balconies for its residents

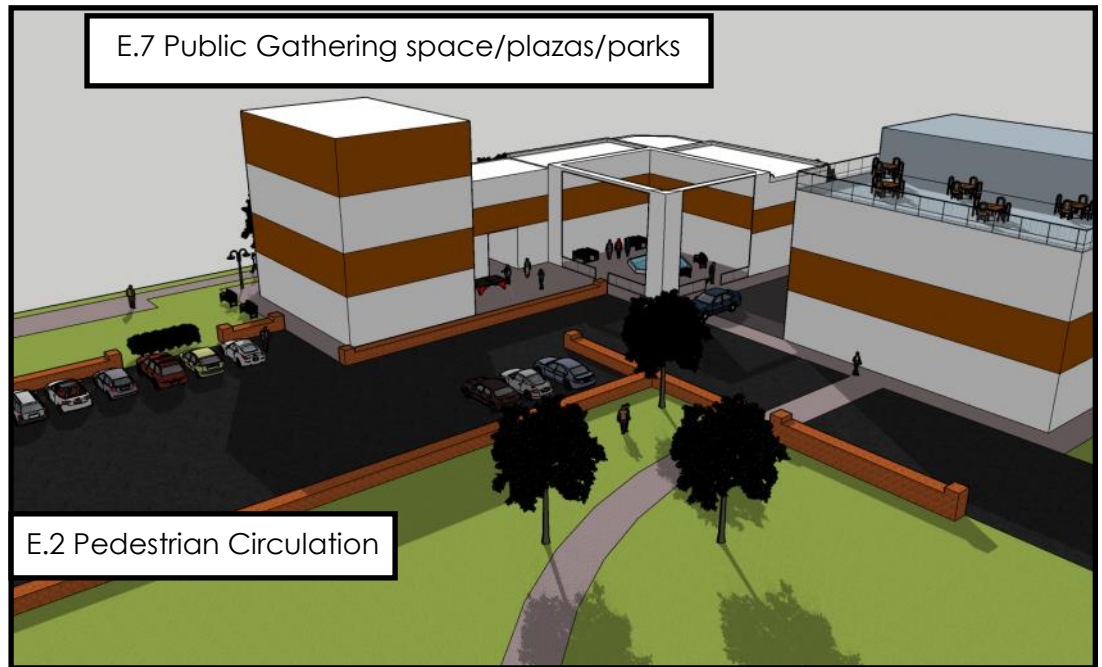
Folkestone and The Promenade work well together in the fact that the businesses that operate on the ground floors in and around Folkestone are often catered towards the older population.

It is the intent of the City West District to encourage developments that are complementary to one another.

E. Recommended Design Guidelines

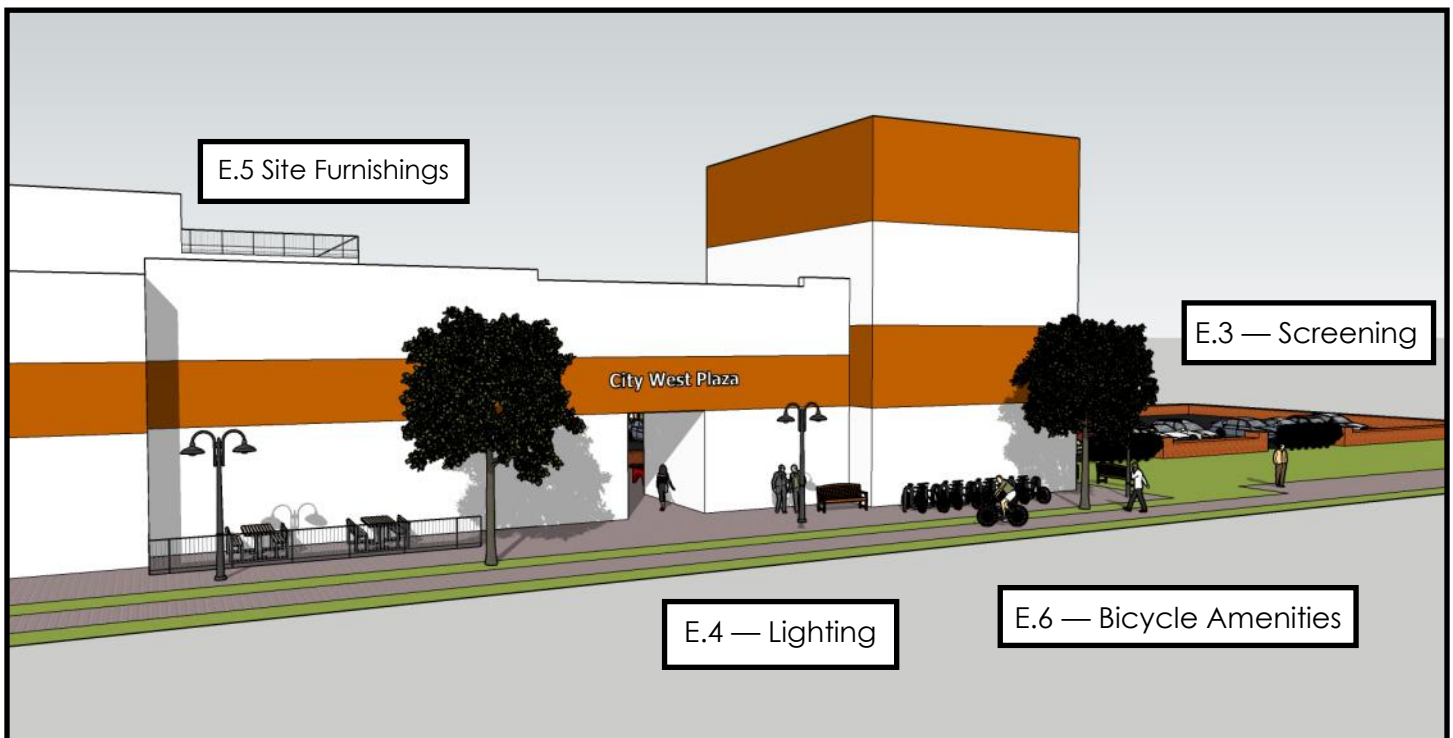
- E.1 General Standards
- E.2 Pedestrian Circulation
- E.3 Screening
- E.4 Lighting
- E.5 Site Furnishings
- E.6 Bicycle Amenities
- E.7 Public Gathering space/plazas/parks
- E.8 Building Design
- E.9 Parking Structures
- E.10 Landscaping
- E.11 Greenway Buffers

Figure E1: model of development with public gathering spaces



Section E — The Recommended Design Guidelines draws language from the Zoning Ordinance to explain design standards that are encouraged to be incorporated into developments in City West. Figures E1 and E2 are models containing certain aspects anticipated to be included in City West sites such as amenities for pedestrians, public meeting areas, signage, lighting, and other details as laid out in this section. The section will break each subsection into **Design Intent**, **Configuration and Scale**, and **Colors and Materials**, where possible.

Figure E2: model with recommended design guidelines



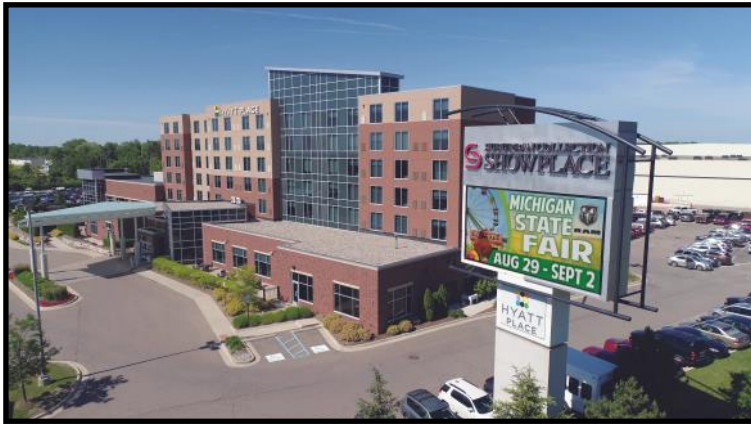
E. Recommended Design Guidelines

E.1 — General Standards

Design Intent

The proposed uses will **complement and support the intent** of the City West district, the convention center and other established uses within this area of the City.

Proposed projects will demonstrate that **design coordination and connection with adjacent property**, developed or not, has been accomplished.



The Suburban Collection Showplace brings numerous visitors to Novi every year. The uses in the City West District are meant to supplement the visitors and guests of the city with entertainment and restaurant options, and other complementary uses, within a walkable distance.

Figure E3 (left): The Hyatt Hotel at the Suburban Collection Showplace in

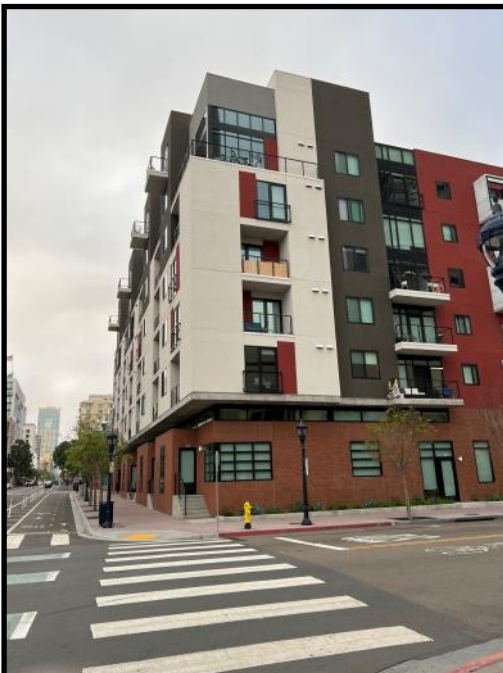
Configuration and Scale

An **attractive street-level environment** has been achieved that focuses on the **pedestrian experience** and provides public space amenities

Vehicular safety provisions and controls have been applied particularly with regard to access to the surrounding street network



Figure E4: splash pads in Carmel, IN



Colors and Materials

Aesthetic quality is improved in terms of design, exterior materials and landscaping, including internal compatibility within the development as well as its relationship to surrounding properties to accommodate future development

Figure E5: apartment building in San Diego, CA with high-quality aesthetic design and exterior materials

E. Recommended Design Guidelines

E.2 — Pedestrian Circulation

Design Intent

Development will be pedestrian-oriented, with a high degree of transparency on the ground level and functional, attractive outdoor amenities such as plazas, seating and lighting. Parking is placed to the side or rear of main buildings, driveways are consolidated on the major thoroughfare, and on-street parking is permitted on secondary streets.

Scale and Configuration

All buildings shall be oriented parallel to the primary pedestrian street with direct entrances and windows facing the street.

Color and Materials

Entrance doors are encouraged to include architectural details such as contrasting paint or material colors and decorative trim.

On-street parking permitted on secondary streets

Windows and direct entrances oriented parallel to the primary pedestrian street



Figure E6: townhomes in Carmel, Indiana

Attractive outdoor amenities such as plazas, seating and lighting

Figure E7: pedestrian amenities in Carmel, Indiana

NOTE: MDO Projects with a single use may be considered for density reserved for mixed-use projects if there is a clear relationship between complementary uses, **including if direct pedestrian connections and buildings entrances are located within 300 feet measured along the pedestrian route (3.34.2.B)**

Signage and clear delineation of pedestrian-routes



Figure E8: crosswalk at a residential development in Novi

E. Recommended Design Guidelines

E.2 — Pedestrian Circulation

Design intent

Site amenities shall create tidy and well-landscaped plazas and pedestrian walkways with decorative paving, pedestrian-scale lighting, seating, trash receptacles, landscape treatments, and attractive architectural features at entranceways and focal points of the development (e.g. arch, gateway, bell tower, fountain, public art). All such amenities shall be privately owned and maintained and shall be included in an open space maintenance agreement.



Figure E9: model with fountain, pedestrian walkways, and seating

Scale and Configuration

Buildings shall be oriented on the property to emphasize a continuous street presence and provide greater pedestrian access and circulation to the site. The main entry to each building shall be from the primary public streetscape, with one entry for every 75 feet of façade along the front lot line. Alongside lot lines, functional entries are required every 100 feet, or fraction thereof, where side-walks are present.



Figure E10: model showing one entry every 75 feet

Street trees should be planted to define the pedestrian corridor and buffer traffic lanes. It will be necessary to understand mature tree height when trees are located within utility easements.

The design of pedestrian walkways and non-motorized paths shall be reviewed with the goal of achieving **connectivity and safety**, and also considering the objectives and intent of this district.

E. Recommended Design Guidelines

E.3 — Screening

Design Intent

Developments which abut off-site single family residential zoned property, shall incorporate noise reduction and visual screening provisions such as earthen and/or landscape berms and/or decorative walls, which shall be approved as to design and location. There is a perimeter setback and screening for the purpose of **buffering the development** in relation to major highways or adjacent residential developments. Developments should include proper setback, landscaping, and massing to address privacy and compatibility with adjacent developments or land.



Figure E11: Huntley Manor Apartments in Novi has screening that separates Grand River, a major arterial, from residential units. Both decorative walls and landscaping in the form of trees were used to screen the homes from the street.

Scale and Configuration

Surface parking lots shall be screened from all major thoroughfares by either (1) a two and one-half (2.5) foot ornamental brick or stone wall with intermittent landscaped breaks, or (2) a semi-transparent screening such as brick pilaster with metal decorative fence in order to maintain attractive streetscapes as approved by the City's Landscape Architect.

The figures below show the various screening treatments. *Figure E12* shows a 2.5-foot brick or stone wall with a landscape break. *Figure E13* shows a brick and metal decorative fence.

Figure E12: brick wall with landscape break

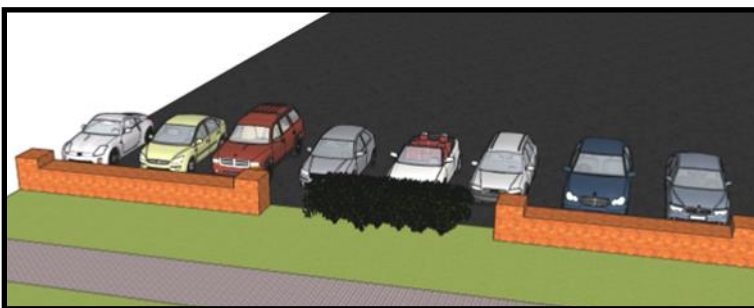
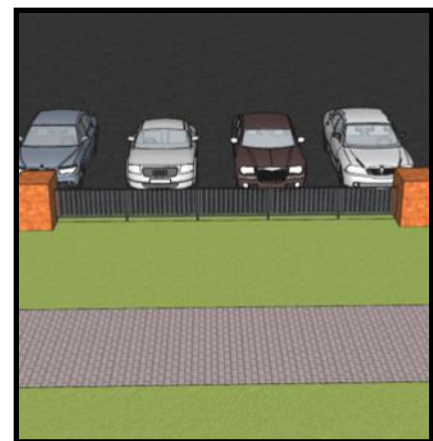


Figure E13: brick with metal fence



E. Recommended Design Guidelines

E.4 — Lighting

Design Intent

Signage, lighting, streetscape, landscaping, building materials for the exterior of all structures, and other features of the project, shall be designed and completed with the objective of achieving an **integrated and controlled development**, consistent with the character of the community, surrounding development or developments, and natural features of the area.



LED Roadway Series; source: DTE Outdoor Lighting Catalog



LED Teardrop Series; source: DTE's Outdoor Lighting Catalog

Configuration and Scale

Height of light fixtures will depend on location in the district. The examples on the left include the LED Roadway Series, meant for local roadways and walkways, and the LED Teardrop Series, meant for downtown districts, plazas, and commercial developments.

Colors and Materials

Light fixtures shall take recommendation from DTE's latest Outdoor Lighting Catalog to determine both light fixture and light pole.

Lighting and landscaping provided along pathway to create a controlled environment. Lighting along pedestrian pathways is essential in maintaining a friendly, safe environment at all times of day.



Figure E14 (below): street view of the Promenade in Wayzata, Minnesota

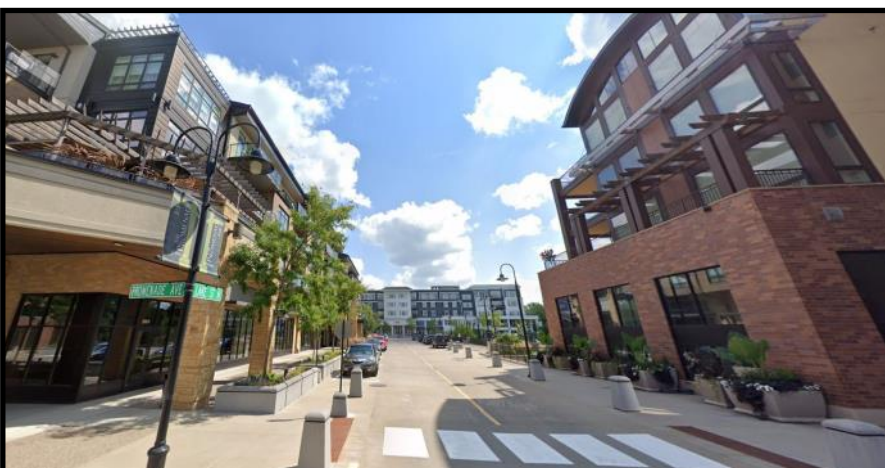


Figure E15 (above): Monon Greenway in Carmel, Indiana.

The development to the left provides ample lighting in addition to porticoes to shelter pedestrians from inclement weather when they walk from place to place

E. Recommended Design Guidelines

E.5 — Site furnishings

Design Intent

Site amenities shall create tidy and well-landscaped plazas and pedestrian walkways with decorative paving, pedestrian-scale lighting, seating, trash receptacles, landscape treatments, and attractive architectural features at entranceways and focal points of the development (e.g., arch, gateway, bell tower, fountain, public art). All such amenities shall be privately owned and maintained.



Figure E16: the receptacle shown to the left is encouraged to be used as a site furnishing (DuMor receptacle 107—black)



Figure E17: the bench shown above is used in parks and along sidewalks within Novi

E.6 — Bicycle amenities

Design Intent

Bicycle parking shall meet or exceed the standards set forth in Section 5.16 Bicycle parking facility requirements. [Figure E18](#), shown below on the left, exceeds the standards by providing bicycle fix-it stations in addition to the standard bicycle parking.



Figure E18: bicycle parking and fix-it station along the Monon Trail in Carmel, Indiana



Figure E19: bicycle parking outside of the Civic Center in Novi

E. Recommended Design Guidelines

E.7 — Public gathering space/plazas/parks

Design Intent

Open Space. A minimum of fifteen (15) percent of the gross site area of a commercial or mixed-use development shall be devoted to publicly accessible, permanently landscaped usable open spaces and pedestrian plaza areas (such as internal walkways, linear or pocket parks, plazas, or other space for public gatherings accessible to non-residential occupants and invitees). Common open space and amenities that **enrich the lives** of the residents should be incorporated.

Configuration and Scale

Small strips of landscaped area, landscaped end islands, and similar areas that are less than twenty (20) feet wide shall not be counted toward the required open space on the site. Up to 50% of the required open space may consist of areas that are regulated by City of Novi woodland and/or wetland ordinances **if they are permanently protected by conservation easements**. Wherever natural open spaces shall be created or preserved, they shall be **physically connected**, when possible, to adjacent natural open space areas.



Figure E20: Huntley Manor Apartments in Novi



Figure E21: Gateway Townhomes in Novi

Figure E22: Farmer's Market in Northville; source: Northville Chamber of Commerce



E. Recommended Design Guidelines

E.8 — Building design

- A. All developments with two or more buildings shall be designed with **variation between building setbacks** and/or placement to avoid the creation of monotonous streetscapes. Site plans shall be designed with variation in both the patterns and the siting of structures so the appearance of the streetscape is not repetitive. The following design guidelines shall be considered:
- B. Varying roof lines and forms: provide offsets or breaks in roof elevations of two (2) feet or greater in height. Incorporate roof pitch and materials of adjacent buildings into carport or garage roofs.
- C. Building façade colors and materials: **At least two variations in color and materials** should be used in the treatment of the building façade. Decorative patterns on exterior materials may include scales/shingles, wainscoting, ornamentation, and similar features that incorporate texture and interest.
- D. **Bays, porches and balconies:** Incorporate smaller-scale forms such as bays, recessed or projecting balconies or porches and dormers into the design to visually reduce the height and scale of the building and to emphasize the definition of individual units.
- E. Wall offsets: A wall offset is defined as a projection or recess of a façade wall of at least two (2) feet in depth. Wall offsets should be incorporated onto those building facades having a length of 40 feet or greater. Wall offsets should be a minimum of six (6) feet in length.
- F. Windows: **Decorative window features**, such as arched windows, transoms, decorative pediments, and/or window shutters that are sized appropriately and proportionally to the window are encouraged on those facades fronting on streets. The use of recessed windows, moldings, decorative trim and wood frames to add three-dimensional quality and shadow lines to the façade.
 - On retail buildings, windows within areas of the premises to which the public is invited shall be mostly transparent.

Additional conditions for Townhomes:

- No more than 8 attached units per buildings
- Minimum distance between buildings of 20 feet



Figure E23: townhomes by Caliber Homes of Ontario

E. Recommended Design Guidelines

E.8 — Building design

Design Intent

Townhouse and apartment dwellings are **designed to be compatible** with surrounding neighborhoods and land use

Configuration and Scale

Structural placement, building orientation, and landscaping treatment along the streetscape and open space areas comply with design guidelines. Site design guidelines orient and cluster buildings and design parking, landscaping and open space in ways that **connect to surrounding neighborhoods and complement view-sheds** and surrounding natural features.

Figure E24: (right) Townhomes as residential infill in Traverse City, Michigan

Compatibility and connectivity are recommended to take high priority in site design. *Figure E24* to the right shows a clustering of residential buildings that frames the street. The plaza in *Figure E25* below sits in front of an office building and can serve as a meeting place for those who live and work in the area.

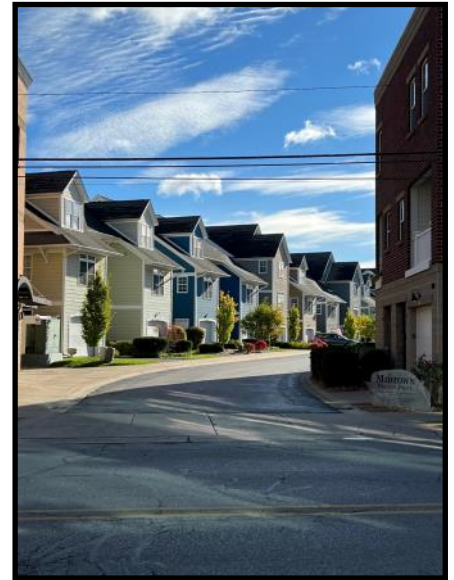


Figure E25 (below): rendering of a plaza in Virginia Square in Arlington, Virginia; source: arlnow.com



E. Recommended Design Guidelines

E.9 — Parking Structures

Design Intent

Buildings are encouraged to have parking on the ground level or in structured garages. Structured parking within or under the building shall be aesthetically and effectively screened through architectural design, landscaping, or other means, from adjacent drives, walkways, and buildings, and particularly from the street level view.

Color and Materials

An architectural treatment, such as a finished fascia, shall be provided to shield any unfinished structural elements (including electrical elements, exposed metal beams, and fireproofing material) or mechanical appurtenances from a viewing position at grade from the opposite side of the street.

Scale and Configuration

The perimeter of each parking structure floor above street level shall have an opaque screen or other screening mechanism to shield automobiles from public view. The screen shall be at least 3½ feet high measured from the finished floor elevation.



Figure E26: Parking structure with screening and high quality material in Ann Arbor



Figure E27: Parking structure in Traverse City

The top floor of parking structures that are open to the sky are subject to landscaping requirements for parking areas as described in subsection 5.5.3.C.

35% of parking structure frontage shall have foundation landscaping (living walls could meet that requirement)

E. Recommended Design Guidelines

E.10 — Landscaping

Design Intent

The intent of the landscaping in City West is to promote walkability, create welcoming environments, and establish public areas where people may gather. Many aspects of the landscaping should be uniform across developments to promote the identity of a single district. City West is meant to be more urban in character, thus the landscape standards are less stringent compared to other areas of the City. Standards differ between commercial/mixed use buildings and strictly multifamily residential. See table 3.34.6.E in the ordinance text for mixed-use and commercial building requirements.

Configuration and Scale

In areas where the surface is made up primarily of concrete, box planters and other urban style planters can be used in front of each building, with occasional larger beds to provide visual interest. Planters should be placed to frame seating areas and in front of buildings. The quantity of trees planted shall be based on building frontage, but will be more flexible in order to prioritize healthy tree growth. For example, if steps are taken to ensure the longevity and health of planted trees, then the required quantity may be lessened. An example of this includes the use of silva cells with permeable pavers (seen below in [Figure E28](#)), or an approved equal alternative that encourages trees' survival with minimal open area.

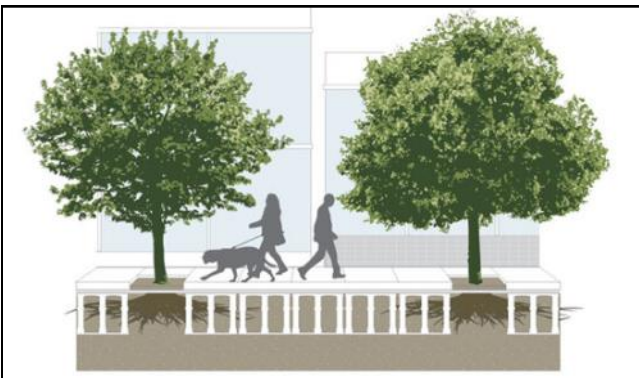


Figure E28: illustration of silva cells underneath urban trees; source: landscapeperformance.org



Figure E29 (above): image of tree guards in New Zealand; source: urbaneffects.co.nz

Figure E30 (left): image of a linear pocket park in between townhomes near Richmond, VA

E. Recommended Design Guidelines

E.10 — Landscaping

Flexibility

To allow for the necessary flexibility of urban landscape design, new landscaping standards have been established for both the residential, nonresidential, and mixed-use developments in City West. The new standards generally lessen the amount of landscaping required, including fewer canopy trees per dwelling unit, smaller greenbelts, and flexibility for paved open areas to include features such as movable planters to satisfy landscaping requirements.

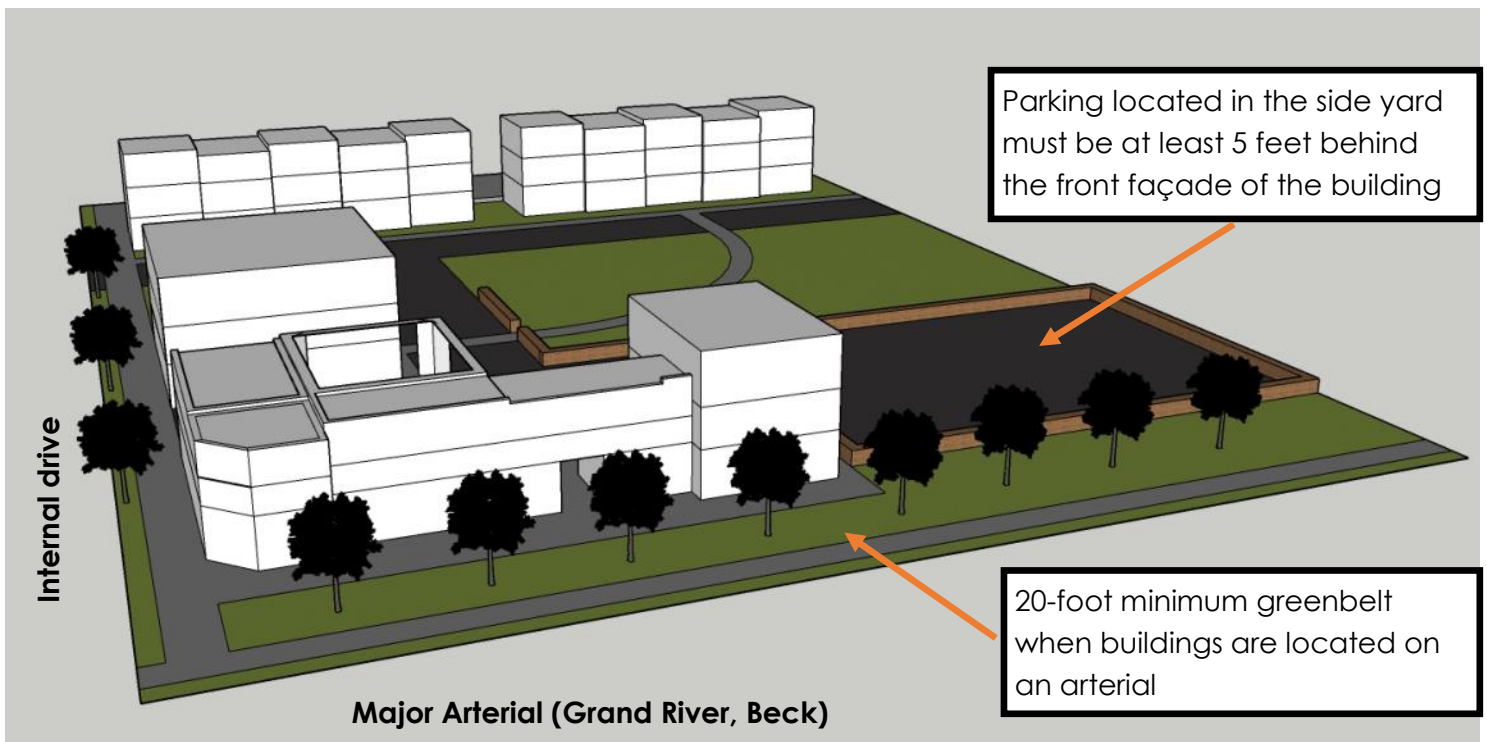


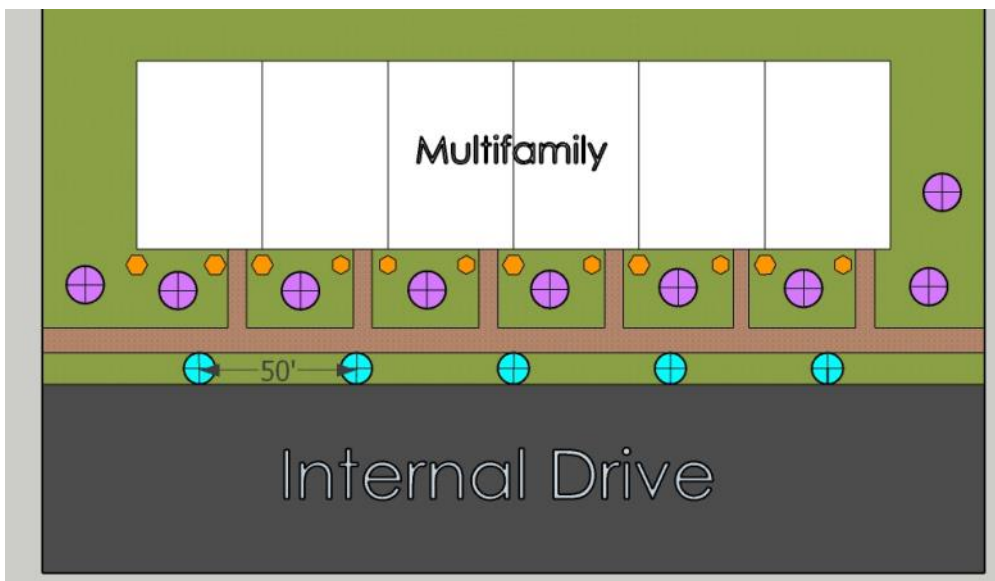
Figure E31: model showing some new landscaping standards for City West

Greenbelt on an arterial (Grand River, Beck)

The greenbelt must meet a minimum 10 foot requirement if adjacent to parking and a 20 foot requirement if not adjacent to parking. See Figure E31 above. Even spacing of canopy and subcanopy trees is required. See Table 3.34.6.E Landscaping Requirements for Mixed Use/Commercial Buildings in the ordinance text for more detail.

E. Recommended Design Guidelines

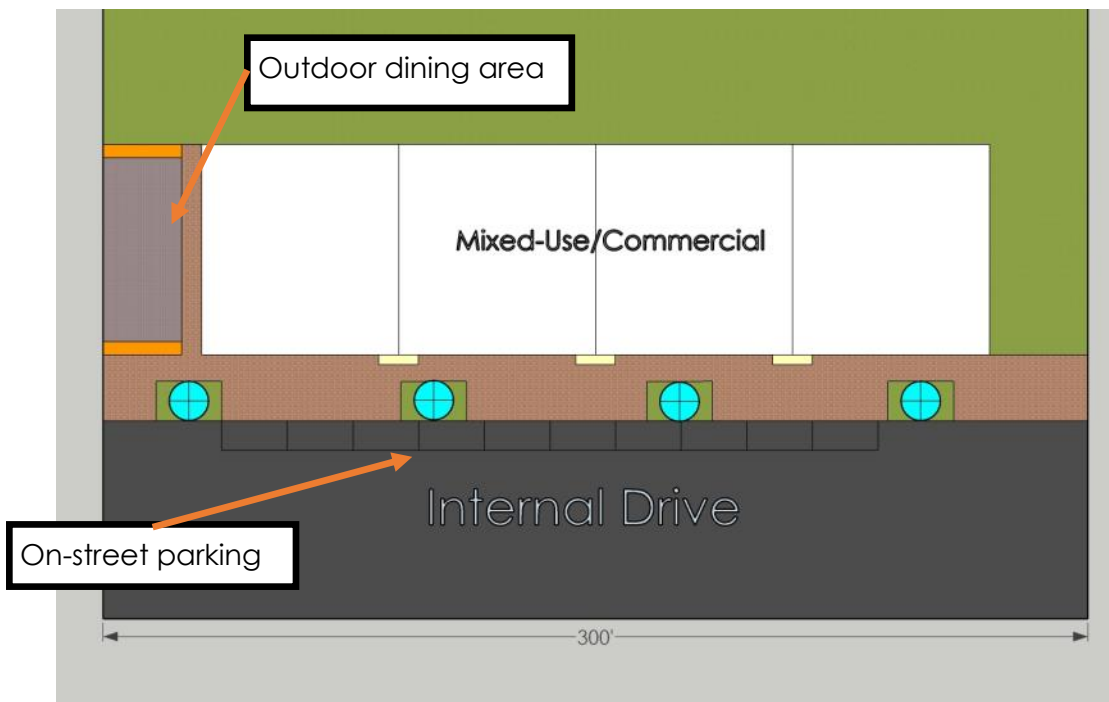
E.10 — Landscaping



For buildings that are standalone multifamily uses, the tree count will be based on the number of first livable floor units at **1.5 trees per unit**, in addition to **2 shrubs per first floor unit**. Interior street trees shall be placed **one per 50 linear feet**.

- 6 units x 1.5 trees/unit = 9 trees
- 6 units x 2 shrubs/unit = 12 shrubs
- Canopy trees every 50 linear feet, evenly spaced

Figure E32: landscaping standards model for multifamily



Commercial standalone buildings or mixed-use buildings with commercial on the first floor in City West have more urban landscaping standards than other similar uses throughout the City. For internal drives, there shall be **1 canopy tree per 75 linear feet**, spaced not closer than 25 feet apart from each other and planted in silva cells or equivalent substitutes to ensure tree health.

Figure E33: landscaping standards model for mixed-use/commercial

- Canopy trees every 75 linear feet, spaced not closer than 25 feet
- 60% of periphery landscaped for outdoor dining area (permanent beds or movable planters)
- If sidewalk 12 ft or wider, 15% of building frontage shall have foundation landscaping 2-3 feet deep

2016 MASTER PLAN FOR LAND USE

EXCERPT

2016 Master Plan Goals

1. **Quality and variety of housing.** The City of Novi is known for its high-quality residential neighborhoods and should strive to ensure the availability of a wide range of attractive housing choices that are protected from noise, traffic, and other impacts of non-residential development. Encourage the development of neighborhood open space and neighborhood commercial goods and services to minimize motorized travel.
2. **Community identity.** The City's identity is largely based on its high-quality residential neighborhoods and schools, destination retail and convention space, and its parks. The City should supplement that identity by enhancing the preservation of the City's historic resources and expansion of its cultural opportunities. New development of land should continue to be of high-quality design and materials.
3. **Environmental stewardship.** The City of Novi is significantly enhanced by the preservation of natural resources in both residential and non-residential areas. Maintain public and private stewardship of the natural environment through the preservation of open space, protection of woodlands and wetlands, and utilization of low-impact development techniques.
4. **Infrastructure.** Invest wisely into the ongoing maintenance and improvements to existing infrastructure, including utilities and the transportation network. Ensure that new development minimizes the demands placed on the City's existing infrastructure. Support the City's entire transportation network through the development and enhancement of non-motorized transportation facilities and amenities.
5. **Economic development.** The City's developed land, infrastructure, and natural resources are interconnected and collectively impact the daily lives of the City's residents and business owners. The City should strive to maintain the balance between the economy, the environment, and the community to ensure sustainable development that meets the needs of today while ensuring that the needs of future generations can be met.

Objectives

Objectives are grouped by general goal. In many cases, the objectives align with more than one goal. In the Implementation chapter, these alignments are provided as cross-references.

General Goal: Quality and Variety of Housing

1. **Southwest Quadrant.** Maintain the semi-rural character of the southwest quadrant of the City that is created by low-density residential development and undeveloped land.
2. **Twelve Mile-Napier-Wixom Roads Study Area.** Develop the Twelve Mile, Napier, and Wixom Roads Study Area, as identified in the 2008 Master Plan, with a mix of residential, educational, and commercial uses in an aesthetically pleasing manner that is compatible with neighboring land uses and complements the natural environment.
3. **Provide residential developments that support healthy lifestyles.** Ensure the provision of neighborhood open space within residential developments.
4. **Safe housing and neighborhoods.** Enhance the City of Novi's identity as an attractive community in which to live by maintaining structurally safe and attractive housing choices and safe neighborhoods.
5. **Maintain existing housing stock and related infrastructure.**
6. **Provide a wide range of housing options.** Attract new residents to the City by providing a full range of quality housing opportunities that meet the housing needs of all demographic groups including but not limited to singles, couples, first time home buyers, families and the elderly.

General Goal: Economic Development / Community Identity

7. **I-96/Novi Road Study Area.** Develop the I-96/Novi Road Study Area in a manner that reflects the importance of this important gateway to the City in terms of its location, visibility, and economic generation. Mitigate impacts to the City's infrastructure. (See page 125)
8. **City West/Grand River & Beck Road.** Develop the City West/Grand River Avenue and Beck Road area in a manner that supports and complements neighboring areas. (See page 126)

9. **Pavilion Shore Village.** Develop a cohesive mixed use village that complements the surrounding neighborhood. (See page 112)
10. **The Anglin Property.** Foster development of a mixed-use district that complements Novi Town Center. (See page 118)

General Goal: Community Identity

11. **Maintain quality architecture and design throughout the City.** Set high standards and promote good examples for use of public property through the City's actions.
12. **Create a stronger cultural presence and identity for the City by working with the Novi Historical Commission and other groups to preserve historic structures and creating gathering places for residents and community activity.**

General Goal: Environmental Stewardship

13. **Protect and maintain the City's woodlands, wetlands, water features, and open space.**
14. **Increase recreational opportunities in the City.**
15. **Encourage energy-efficient and environmentally sustainable development through raising awareness and standards that support best practices.**

General Goal: Infrastructure

16. **Provide and maintain adequate water and sewer service for the City's needs.**
17. **Provide and maintain adequate transportation facilities for the City's needs.** Address vehicular and non-motorized transportation facilities.

General Goal: Economic Development

18. **Retain and support the growth of existing businesses and attract new businesses to the City of Novi.**

General Goal: Economic Development / Community Identity

19. **Ensure compatibility between residential and non-residential developments.**

Future Land Use Descriptions

The **Future Land Use Map FLU-1** is shown on page 47. Land use designation descriptions modified in this 2016 Master Plan Update are indicated in **green text and are referenced in the overall Map FLU-6 at the end of this chapter.**

Single-Family Residential

This land use is designated for single-family detached residential. The recommended density or the number of dwellings per acre varies throughout the City. Higher density residential land use is designated in areas that transition to commercial or light industrial developments. Refer to the **Residential Density Map FLU-2** for specific density recommendations. **The property now developed as Berkshire Pointe has been changed from a designation of Community Commercial to Single-Family Residential.**

Manufactured Home Residential

This land use is designated for housing within a manufactured housing community, created according to the regulations in the Manufactured home Commission Act. Housing in these areas is manufactured in a factory, brought to the site, and in most cases placed on property leased from a park operator.

Multiple-Family Residential

This land use is designated for multiple-family residential dwellings in a variety of settings, ranging from two-family dwellings to low-rise and high-rise apartment complexes. Refer to the Residential Density Map for specific density recommendations.

Suburban Low Rise

This land use is designated for suburban low rise uses including attached single-family residential, multiple-family residential, institutional and office uses when developed under a set of use and design guidelines to keep the residential character of the area and minimize the effect that the transitional uses would have on nearby single-family residential properties.

Community Office

This land use is designated for small- and medium-scale office uses that primarily serve the residents of the City. The area may also include facilities for human care and indoor or outdoor recreation.

Office Commercial

This land use is designated for a variety of medium-scale and large-scale general and medical office buildings or complexes with limited personal service and retail uses. The area may also include facilities for human care, hotels, motels, higher education and indoor or outdoor recreation.

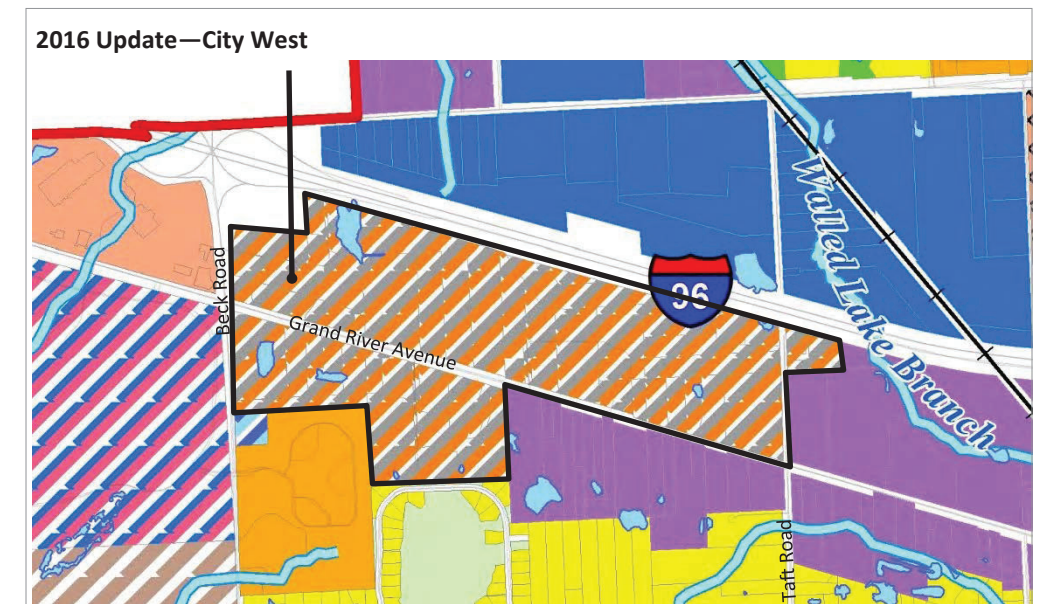
Office, Research, Development and Technology

This land use is designated for a variety of medium-scale and large-scale general and medical office buildings or complexes and research, development and technology facilities, with or without related manufacturing or warehouse facilities. The area may also include facilities for office, research and development support services, human care, hotels, motels, higher education and indoor or outdoor recreation. **In addition, this designation incorporates the former Office, Research, Development, and Technology with Retail Service Overlay (see text that follows) that may allow a limited amount of retail services in appropriate locations to serve the employees and visitors of these use areas, including but not limited to fuel stations, car washes, restaurants (including drive-through) and convenience stores as implemented through the Zoning Ordinance.**

Office, Research, Development and Technology with Retail Service Overlay—Deleted (2016 update) This land is now a part of the City West land use planning category.

City West (2016 update)

This land use is designated to accommodate a mix of uses in a dense, walkable setting. It is anticipated that this area will attract young professionals and empty nesters looking for more dense housing options along with vibrant retail, restaurant, entertainment, and office uses. Typical building heights will range from two to five stories. Additional building height, up to ten stories, is envisioned along the I-96 frontage. This may require structured parking and enhanced pedestrian linkages will be important. A system of local and collector streets will be established to connect sites and provide additional parking. Ground floor retail, restaurant, and entertainment uses are envisioned throughout the area, and should be included in structured parking to maintain an interesting, continuous street life for pedestrians.

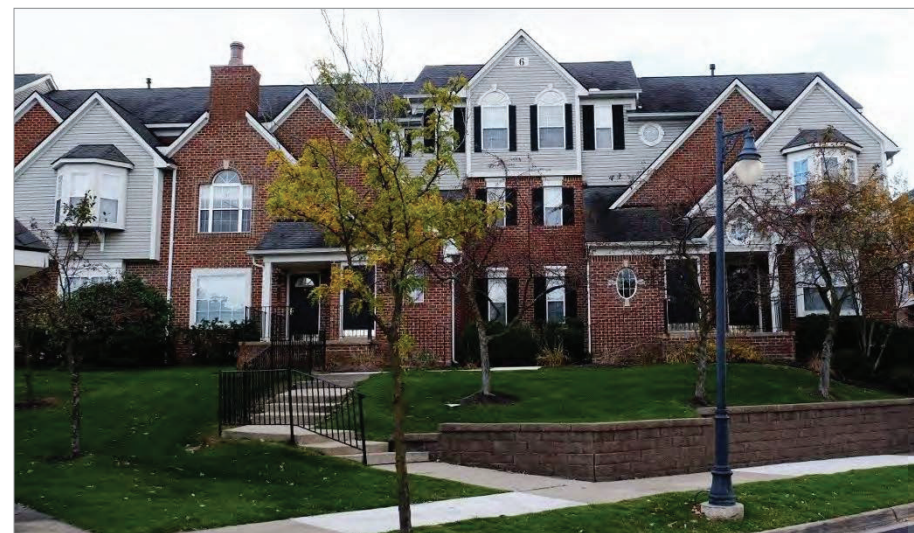


Local Commercial

This land use is designated for convenience shopping for residents within nearby neighborhoods. It includes retail, personal service establishments, and small offices.

In order to provide for a greater variety of housing types, the 2016 Master Plan update provides seven notable changes to the residential density map. These changes are highlighted on **Residential Density Update Map FLU-3** on the following page.

1. **Pavilion Shore Village.** This area, located at Thirteen Mile and Old Novi, is envisioned to accommodate medium-density residential and commercial development and is further described in the Redevelopment Strategies chapter.
2. **Twelve Mile/Meadowbrook.** Density increased from 0.8 to 2.5 dwelling units per acre at the northeast corner of Twelve Mile Road and Meadowbrook Road due to a recent development proposal project that aligns with the City of Novi housing goals and objectives. The residential development plans to support healthy lifestyles by setting aside open space for a trail head, pedestrian and bicycle paths, and a small park, while incorporating natural features with wetland enhancement and a context-sensitive greenbelt along Meadowbrook Road.
3. **PD2.** Not included on the 2004 density map, this area is planned for 20.7 units per acre to permit development that complements the high-intensity commercial uses nearby.
4. **Berkshire Pointe.** This area was once designated for commercial use and has been added to the residential density map to reflect rezoning and development that occurred after the previous plan.



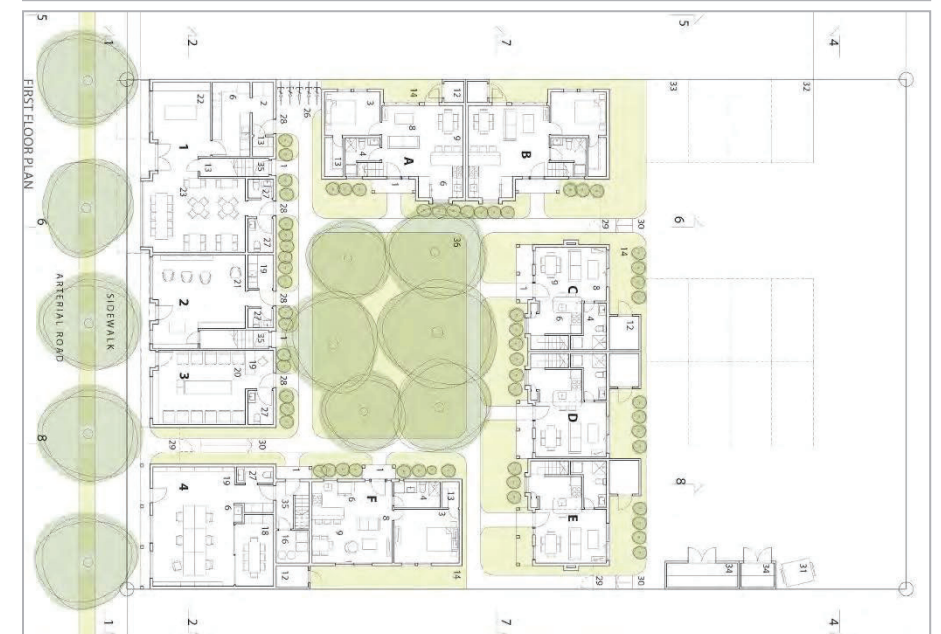
Dense and walkable with access to nearby amenities, Mainstreet Village is an example of Missing Middle housing in Novi.

5. **City West.** The future land use designation City West provides for mixed use development, including a high residential density of 20 dwellings/acre south of Grand River and 30 dwellings/acre north of Grand River near the freeway. This density accommodates the proposed mix of uses including residential in a dense and walkable setting. This area is further described in the Redevelopment Strategies chapter.
6. **TC Commercial and TC Gateway.** Changes to the residential density map reflect the recommendations of the 2014 Town Center Area Study, which calls for dense multiple-family housing in the form of loft-style condominiums, townhouses, or mixed use of commercial, office, and residential buildings. The study also recommended consideration of residential uses on the first floor and live-work buildings in the Town Center.
7. **Nine Mile/Novi Road (northeast corner).** The density of this corner has been increased from 7.3 units per acre to match the adjacent density of 9.3 dwellings per acre.

As noted in the Build-Out Analysis and the **Residential Density Update Map FLU-3** (see following page), high density mixed use areas such as City West and TC Gateway are planned to permit concentrated density in selected locations, but there is a limit to the number of new units based on infrastructure limitations and City policy. See page 49 for residential unit limits in mixed use areas.



The existing Hyatt Place hotel at the Suburban Collection Showplace is similar in scale and impact on utility service as many of the use options proposed for the interstate-adjacent portion of City West.



Images above: This courtyard development with adjacent live-work units designed by Niko Tiula won Opticos Design's Michigan Missing Middle Housing design competition in 2015. It offers higher density and mixed use at a modest scale.

Redevelopment Site 2: City West

Grand River Avenue between Taft and Beck includes the Suburban Collection Showplace, a convention center that plans to expand with additional convention space and an entertainment venue. Other development in this section of the Grand River Corridor is a mix of commercial, office, and industrial uses. Many sites are underutilized, disused, or vacant. This area offers the potential for the creation of a prominent new district combining entertainment, convention, commercial, office, and residential uses in a cohesive, high-density, walkable pattern. Structured parking and diagonal on-street parking along circulation roads will help to reduce the amount of land devoted solely to pavement. This plan envisions three to five story buildings for most of the area, while buildings with frontage on I-96 may rise as high as ten stories.

Components of the District

Residential Uses

The residential components of City West should provide unique housing types, with an emphasis on types currently undersupplied in the City. In general, residential uses will be located above commercial uses, and all residential north of Grand River should be on the second floor or higher. Some ground floor residential uses such as row houses may be appropriate south of Grand River on the edge of the district as a transition to nearby neighborhoods. Live-work units may be an appropriate use as well, particularly if City West develops as an arts and entertainment district. Microapartments (small efficiency units) and small one-bedroom units may be an appropriate and viable development model for this area, especially given the needs of frequent business travelers and temporary residents at nearby Providence Park Hospital.

Commercial, Restaurant and Entertainment Uses

The district is envisioned as a distinct neighborhood as well as a complement to major nearby uses such as the Suburban Collection Showplace and the hospital. Commercial uses in this area could include specialty retail or a cluster of similar uses, such as high-fashion stores or art galleries, in addition to uses that serve nearby residents. Personal service uses would likely constitute a portion of the overall commercial picture in City West. Creating a vibrant restaurant and entertainment scene that spills into outdoor patios and open spaces, is a major goal of this redevelopment strategy.

Office Uses

Office uses in City West should be mostly limited to upper floors and, in vertically mixed buildings, may serve as a transition from retail and restaurant uses on lower levels to residential uses above.



Above, left: Map of City West land use planning area, including footprints of existing buildings. The largest building, at center, is the Suburban Collection Showplace, which is planning to expand. Above, right (clockwise from upper left): public art and gathering space amid high density mixed development in Asheville, NC; high quality building materials and ornamental landscaping in West Bloomfield, MI; finished alley with small retail use in Fort Collins, CO; public plaza and dense, mixed development in Princeton, NJ

Character of Development & Placemaking

For City West to become a true district, development must follow a unified approach. This could be achieved through the development of design standards or a form-based code to establish district-wide standards for building massing and location, streetscape, and public spaces. Building materials, landscaping, lighting, public furniture, and signage can all be addressed in standards for City West. These standards could also establish sub-districts (for instance, north of Grand River versus south of Grand River). To the extent possible, utilities should be buried.

The planned development of the City West district should include a mix of public plazas and parks that provide gathering places, as well as wide sidewalks that can accommodate outdoor dining. Programming of public spaces is encouraged to promote placemaking and strengthen the identity of the district.

Transportation

City West should be a walkable district that accommodates all road users and provides connections to Novi's existing and planned non-motorized pathways. Parking should be a mix of on-street spaces, small surface lots in side and rear yards, and, where density supports the investment, structures. Parking structures may stand alone or be integrated into other buildings, but in all cases, they should be designed to the same standards as other buildings in the district. Ground floor liner uses should be incorporated where possible. Parking plans should consider emerging trends in the automotive market such as the rapidly growing number of plug-in vehicles on the road, as well as the growth of car-sharing.

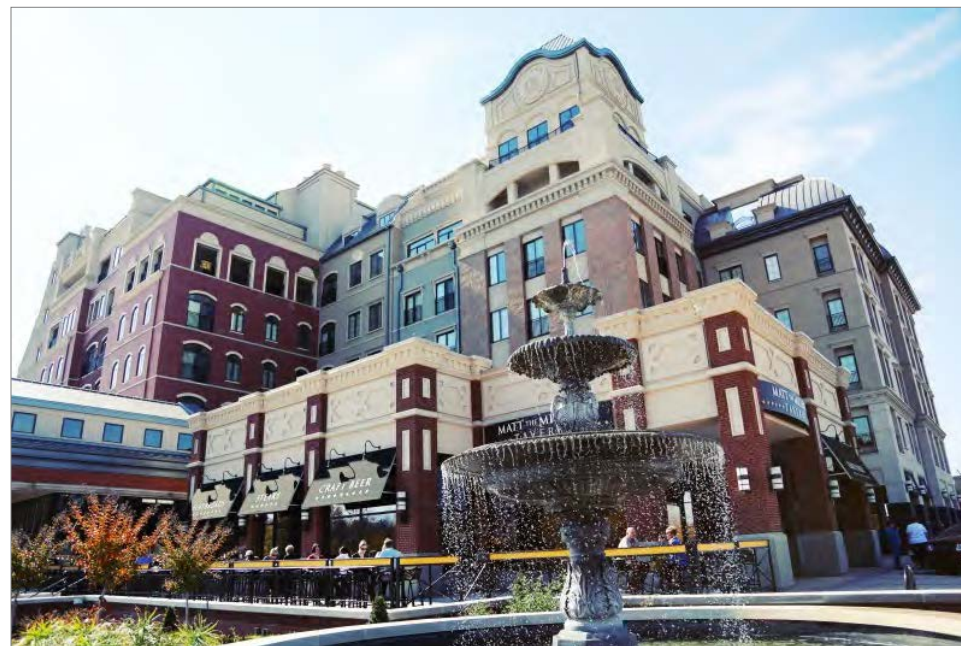
Walkability should be aided by compact development and frequent crossing locations on all internal streets, as well as signal protection at major intersections. Bicycle parking should be provided throughout the district, and sheltered where possible. Finally, if developed to its full potential, City West, with support from the hospital, could play a powerful role in the future development of mass transit on Grand River Avenue, serving as the westernmost destination point for bus rapid transit (BRT). The City should work with its neighbors to determine the feasibility of mass transit to serve the greater corridor and provide expanded mobility options for residents. The cities of Farmington and Farmington Hills have incorporated the potential for BRT along Grand River into their long-range plans.



Source: Roark Premier Team



Source: Elvert Barnes, Wikimedia Commons



Source: Carmel City Center



Source: Central Coast Low Impact Development Initiative

Clockwise from top left: Event in public space, North Hills development, Raleigh, NC, which includes a mix of low-profile buildings and taller structures, including a ten-story hotel; Main North in downtown Royal Oak, MI, is a ten-story mixed-use development with associated structured parking next to two movie theaters; Bioretention swale in Paso Robles, CA, which accomplishes some filtering of runoff before it enters the stormwater system; Carmel City Center in Carmel, IN, is a mixed use development located near the city's downtown Arts & Design District and was developed as part of Carmel's Range Line Road Corridor Plan; Arlington, VA's Pentagon Row project mixes commercial and residential uses around planned, multi-use public spaces.

Sustainability

The development of a new district presents a prime opportunity to consider development from a holistic standpoint. Development of City West should consider mitigating stormwater runoff through bioretention systems such as rain gardens and bioswales, and alleviating concentration of runoff through the use of permeable pavement. Should

development proceed in a coordinated fashion, the City and its partners may consider pursuing LEED-ND certification through the United States Green Building Council; this certification not only provides confirmation that developers have adhered to sustainable development practices, but also serves as a marketing tool for the district. Landscaping with native plants, incorporation of alternative energy systems such as solar collectors or

geothermal heat pumps into building designs, accommodations for electric vehicles, bicycle facilities, and, ultimately, integration with mass transit are all steps that can be taken to build a district that adheres in the long term to basic principles of environmental sustainability. When well-implemented, these measures can also help to limit certain long-term operational costs.



Above: Conceptual rendering of potential development of City West. This image is intended to show building massing, including one ten-story building, and provide an example of how buildings might interact with sidewalks and other public spaces. On-street parking spaces line every block, and most parking is concentrated in structures to the right of the image. Frequent programming of public spaces is encouraged

Development of the District

To facilitate and guide development of the City West district, development of a new zoning classification will be necessary. Form-based standards for the district should be considered as one method of achieving the desired development pattern. The district would likely be more effective if adopted as a stand-alone district rather than as an overlay to existing zoning.



Above: Image, based on development in St. Charles, Missouri, showing the type of streetscape that might characterize City West, along with the three-to-five story buildings envisioned for most of the area north of Grand River. Ground floor retail, space-efficient diagonal parking spaces, and upper floors devoted to office and residential uses combine with an aesthetically pleasing public realm to create a vibrant, pedestrian-friendly district that feels like a cohesive neighborhood.